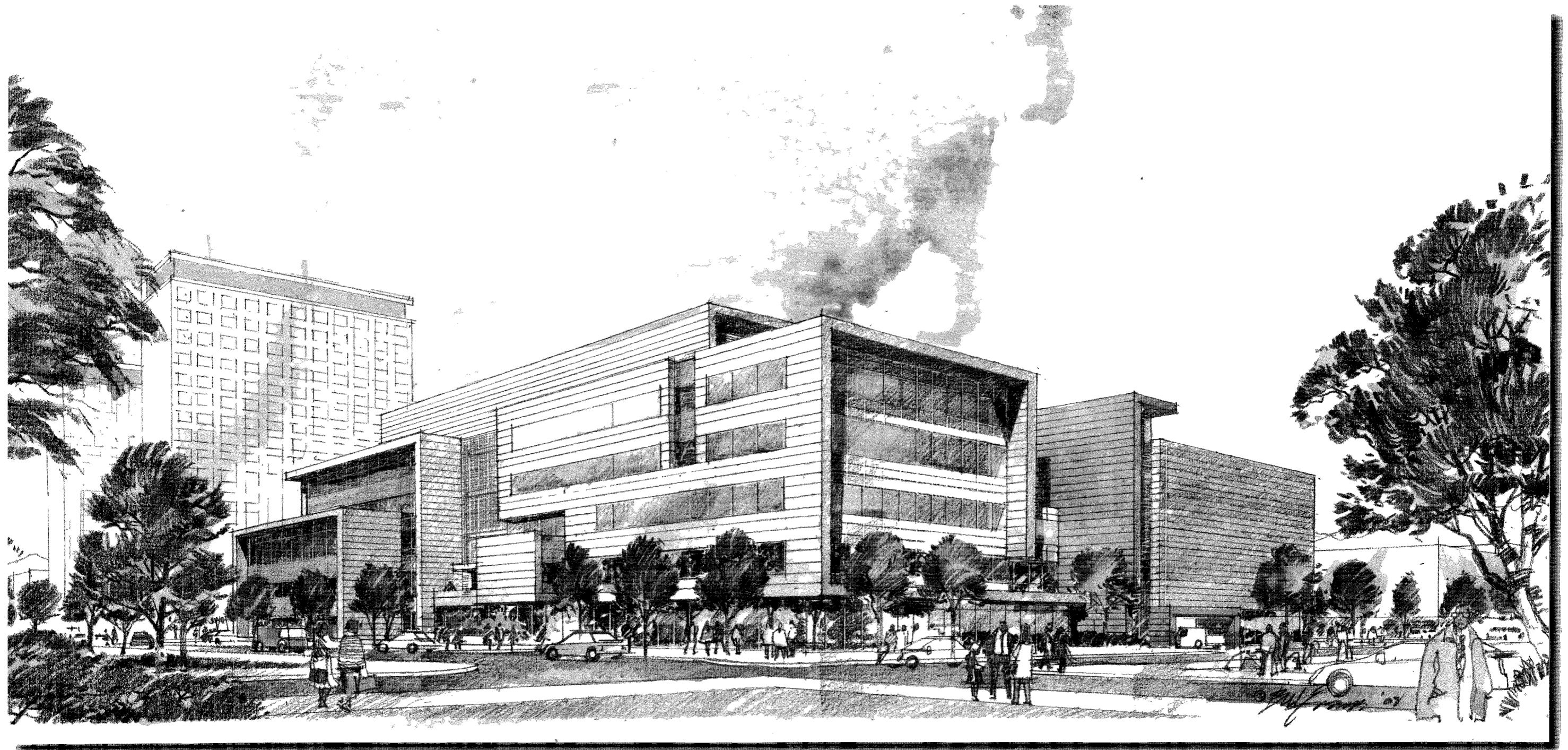
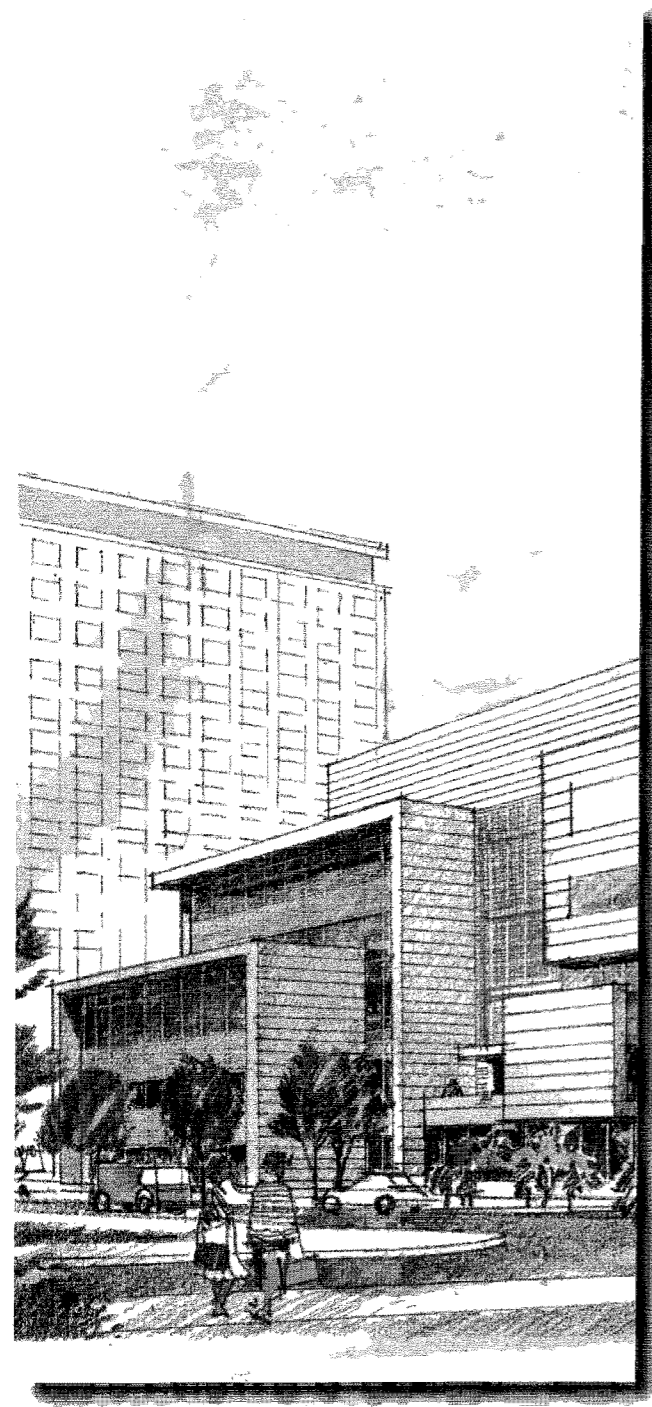


Block 39 Development ■ Office Building and Parking Garage

A New Administrative Office Building and Parking for the Alaska Court System and the Anchorage Legislative Offices





Acknowledgments

Alaska Court System	Contracting Agency
RIM Architects	Architect
RIM Design	Programming and Space Planning
DOWL Engineers	Civil Engineer and Planning Consultant
BBFM	Structural Engineer
Coffman Engineers	Mechanical, Electrical & Technology Engineer
Land Design North	Landscape Architect
Carl Walker	Parking Consultant
Davis Constructors	Cost/Constructability Consultant

Stakeholders

- Alaska Court System
- Alaska State Legislature

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Front cover: project rendering view from 5th Avenue and H Street. Back cover: project rendering view from 4th Avenue and I Street. © Bill Evans.

Project Summary

The Alaska Court System and the Alaska State Legislature have joined together to develop a project to provide administrative office space for Anchorage Legislators and staff, office and warehouse space for court administrative functions, and a parking garage with spaces for employees, jurors and the public. The court system and Legislature received funding in FY08 for the preliminary planning work, which this report summarizes.

Located on Block 39, just south of the Nesbett State Courthouse, the new facility provides a centralized downtown location to serve the Anchorage administrative office needs for both entities. The project will occupy the majority of the block, with the exception of a single property on the northwest corner. The project will include two joined office structures housing the Alaska Court System [ACS] and the Anchorage Legislative Offices [ALO] individually and a parking garage.

This project emerged from unsolicited discussions between the court system, the Municipality of Anchorage (MOA), and the Legislature. During these discussions it became apparent that collectively there might be an opportunity to address each others' issues, creating a beneficial outcome for all. The lease for the ALO is due to expire and, knowing the lead time required, staff had begun to investigate options for future accommodations. It is desirable to consolidate the legislative offices that are inefficiently scattered in various locations around town. The court system benefits from consolidated administrative office space as well as the ability to provide

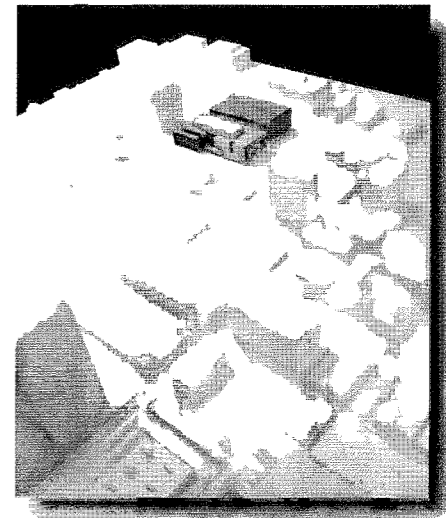
parking for jurors within a reasonable walking distance of the Anchorage courthouses. Finally, the court system was recently made aware that one building on its block 39 property has structural deficiencies perhaps making demolition more cost effective than upgrades.

The new project is an urban friendly building and meets the MOA's goal of keeping government buildings in the downtown core. The parking garage provides parking for employees of both entities, as well as providing convenient parking for citizens called for jury duty. Juror parking when not in use, and other reserved parking after designated hours will be available to meet the public use need in this area of Downtown Anchorage. This parking garage would replace existing parking lots used by the court system, and various leased parking spaces for jurors and the legislative offices.

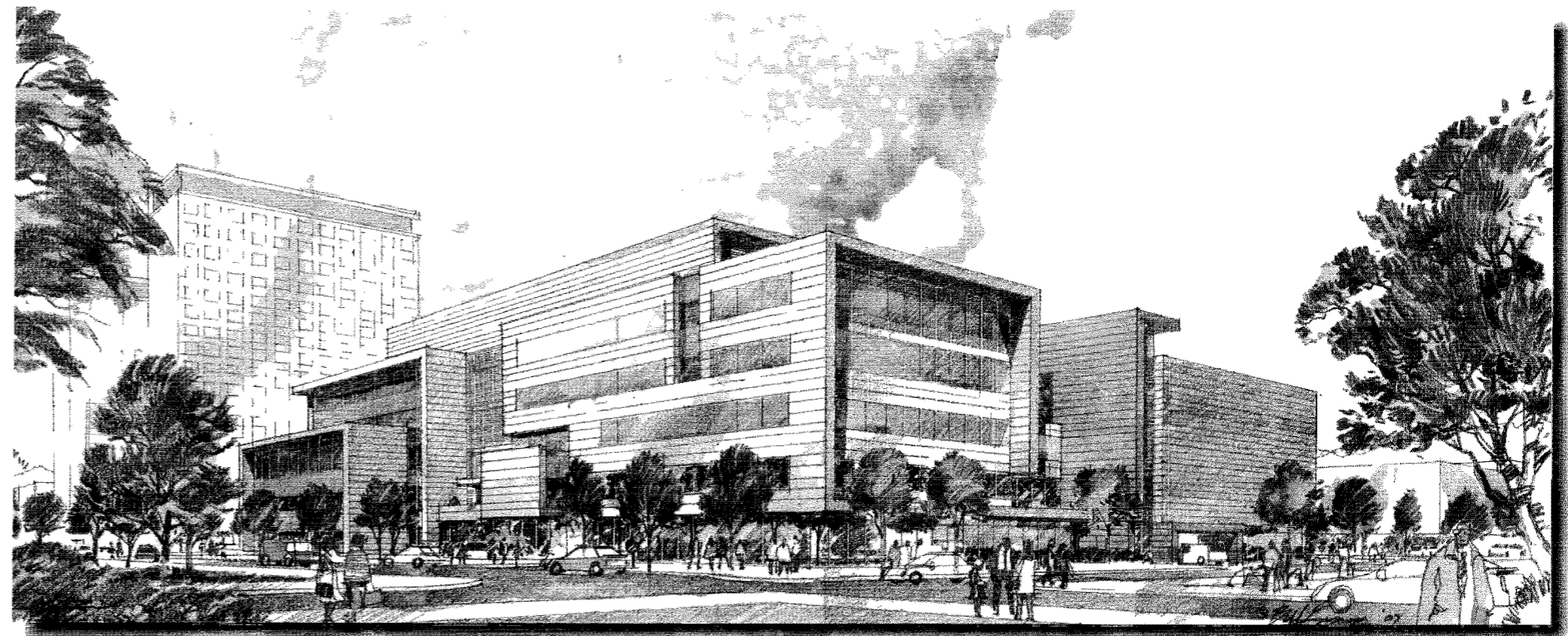
Next Steps

Assuming legislative authorization for this project is gained in spring 2008, ACS plans to immediately initiate a selection process for a design team and a contractor. The project is planned to be financed by the Alaska Housing Finance Corporation [AHFC]. The project implementation goal is to finish design work just after the end of 2008, so that the construction of the office

building can begin in February of 2009, and occupied in early spring of 2010. This schedule allows demolition of the Snowden office building so that construction of the parking garage can begin in April 2010 and be occupied in summer of 2011. This aggressive schedule is beneficial to facilitating project completion efficiently and cost effectively. It also minimizes interim disruption costs, and minimizes risk of project cost escalation.



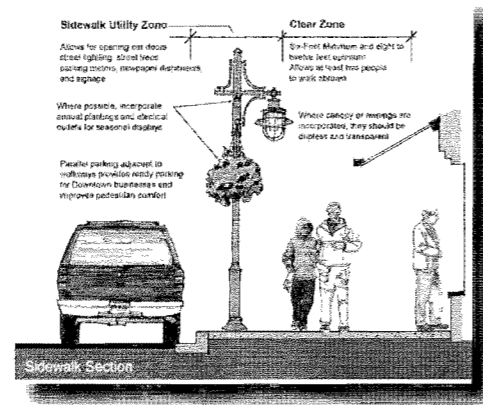
Aerial perspective of sight selected



Project rendering view from 5th Avenue and H Street

Block 39 Development
Office Building and Parking Garage conceptual design

Site Context 1.11.08



Downtown Comprehensive Plan Recommendations

The site is located in the western portion of downtown Anchorage, between 4th and 5th Avenues, and between H and I Streets. The site is south of the Nesbett Courthouse and west of the existing ALO building. The site consists of five of the six lots that make up Block 39. An alley bisects the block and connects H and I Streets.

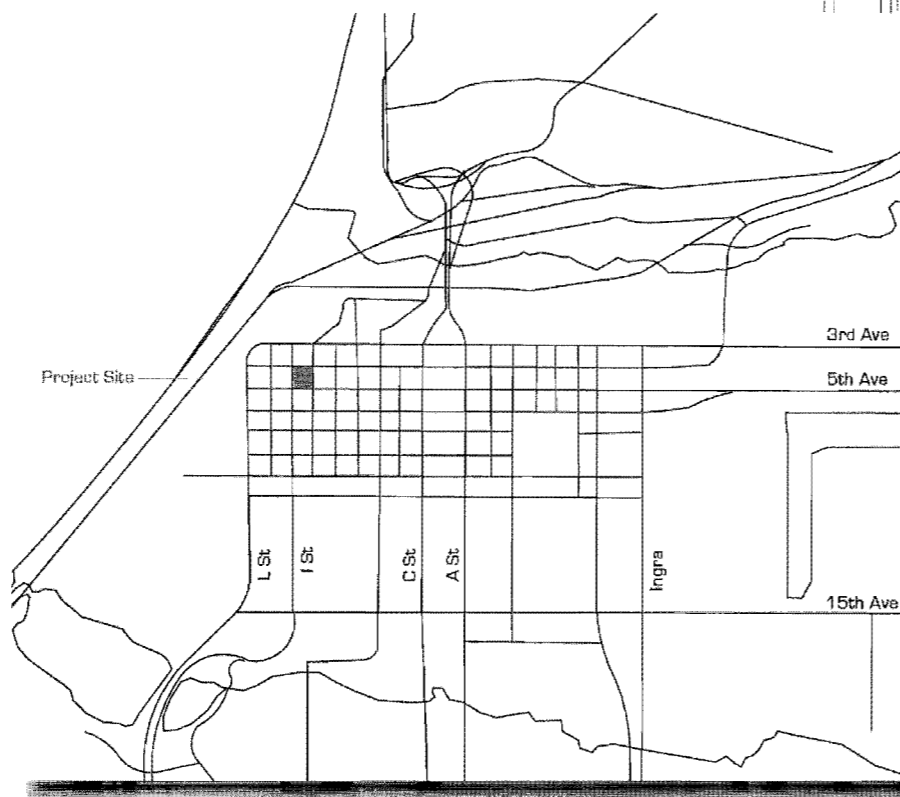
Access to Block 39 is via primary corridors I Street and 5th Avenue; one-way streets flowing north and west respectively. Both H Street and 4th Avenue are two-way travel streets. Fourth Avenue, H Street, I Street and the alley are within MOA Right-of-Ways. Fifth Avenue is within an Alaska Department of Transportation & Public Facilities [AKDOT&PF] Right-of-Way.

Along with the Nesbett Courthouse north of the site, the ACS also occupies the Boney Courthouse which is located on the block northwest of the site. On the block to the west is the Captain Cook Hotel. The Holy Family Cathedral and associated buildings occupy the block south across 5th Avenue. Several restaurants, retail shops, and office buildings are located on the balance of blocks to the southeast, southwest, west and northwest.

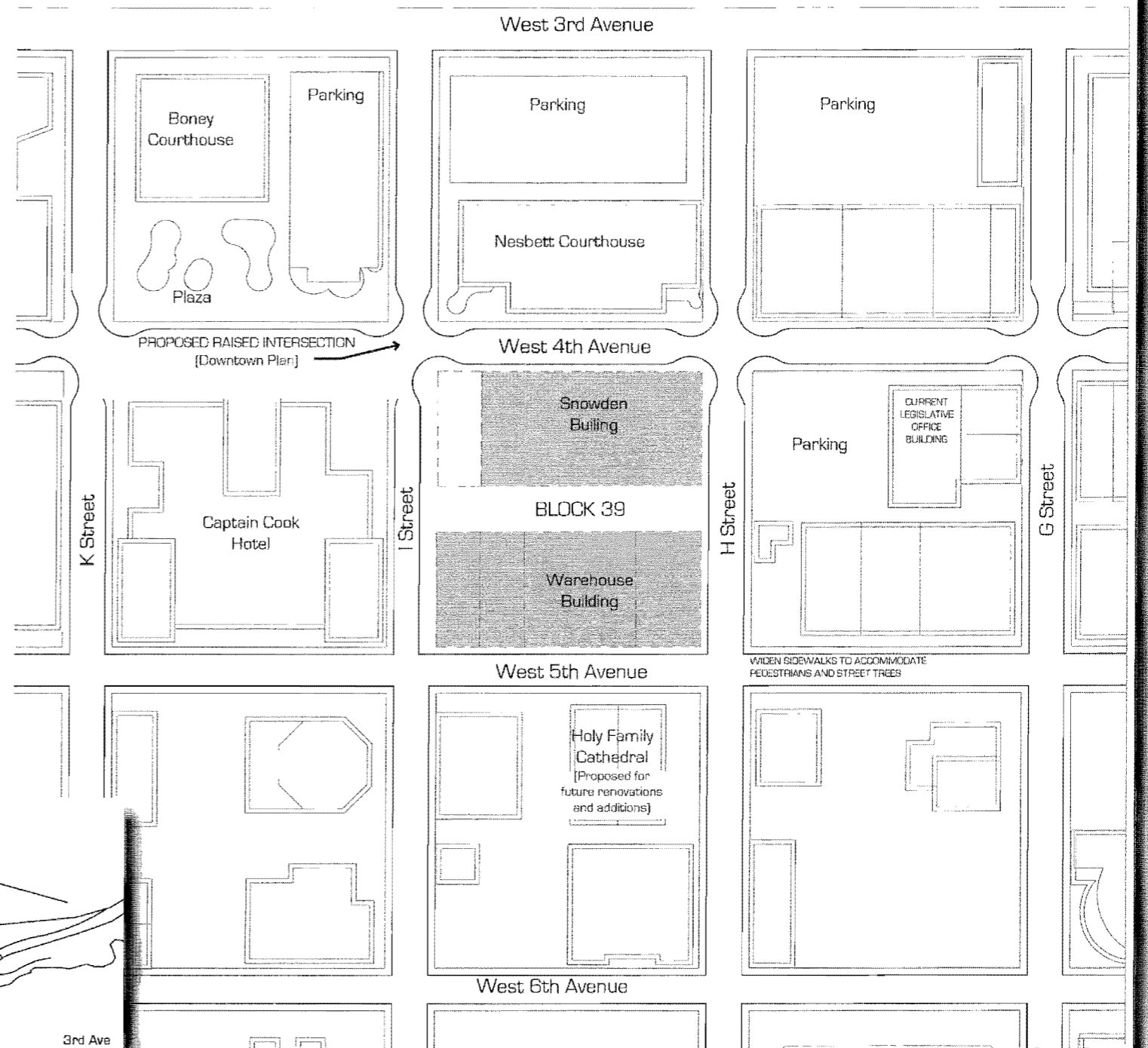
The site is within the local zoning district B-2B "Central Business District - Intermediate", and is within the recently adopted "Anchorage Downtown Comprehensive Plan."

The Comprehensive Plan identifies 4th Avenue as a significant pedestrian route. It recommends the sidewalk be heated and that curb bulbs be located at

the intersections. The Plan suggests that pedestrian circulation at intersections can be improved by raising the intersections and moving the storm drain catch basins towards mid-block. Furthermore, the Comprehensive Plan encourages building setbacks to permit wider sidewalks, and development of "view-sheds and vantage points" from upper levels of buildings.

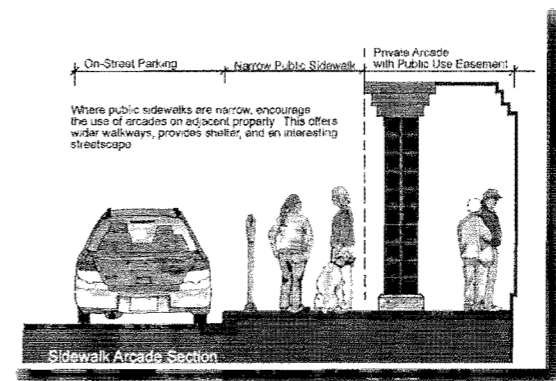


Greater Anchorage area site context



PROPOSED RAISED INTERSECTION
(Downtown Plan)

WIDEN SIDEWALKS TO ACCOMMODATE
PEDESTRIANS AND STREET TREES



Downtown Comprehensive Plan Recommendations



Concept Design Process

Due to the aggressive schedule for this project, it was important that this concept design phase be conducted in such a way that the initial three stakeholders; ACS, ASL, and MOA, could be fully informed and able to make key decisions so that design progress could be maintained. To meet this goal, RIM and their design team facilitated a design process centered around periodic interactive design workshops where all stakeholders and the entire design team was present. This allowed every aspect of the project design to be discussed and evaluated in a very collaborative and transparent way and gave the stakeholders all the information needed for competent and timely decision making.

Design Workshop # 1

At Design Workshop #1, the project was kicked off and the overall project goals and objectives were reviewed and updated. This allowed the stakeholders to evaluate current project design criteria and alert the design team of any changes.

Highlights of Design Workshop #1 include:

- Key project goals and objectives were defined and agreements reached.
- An initial draft project program (the summary of space needs), was presented and accepted.
- An overall project schedule and strategy was delineated.
- Site design considerations and constraints were outlined.
- Quantity of parking in the parking garage needed to be defined.
- Quantity and plan for potential available lease space in the building needed to be defined.

November 6, 2007

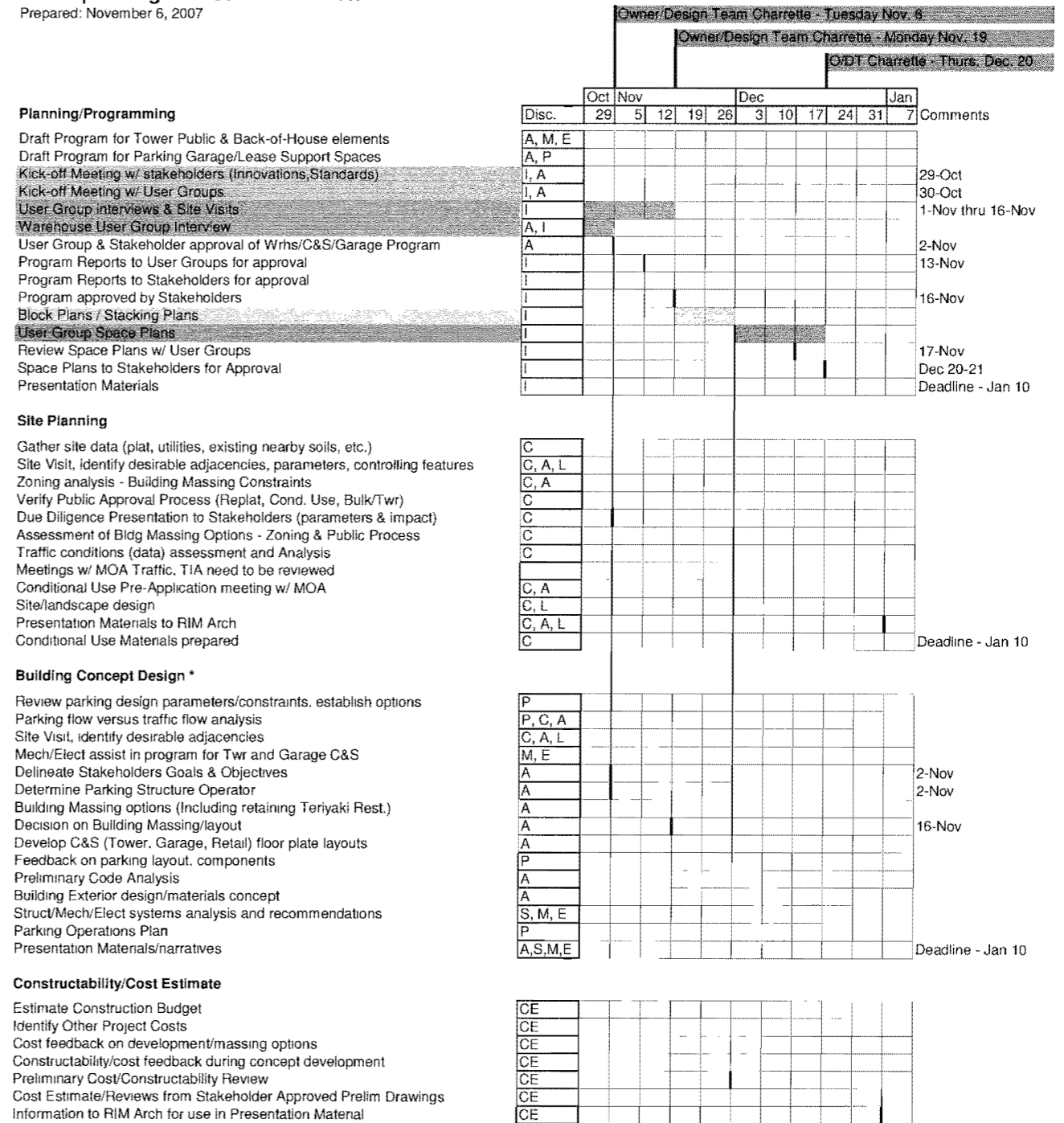
Design Workshop # 1

Understanding of Project Goals and Objectives

1. Provide office space to replace and consolidate current Anchorage Legislative Offices
2. Provide office space to replace and consolidate the Alaska Court System administrative offices.
3. Define MOA lease space for potential retail or office tenants at street level locations, configuration and areas.
4. Achieve independent architecture, access, image and circulation for Alaska Court System and Anchorage Legislative Offices.
5. Provide a parking structure to meet the needs of the stakeholders, and the vicinity parking count/ operations/ configuration to be defined.
6. Create an urban friendly building meeting intent of the Downtown Comprehensive Plan and Title 21.
7. Define common vs. separate building systems through concept design.

Concept Design Process Work Plan

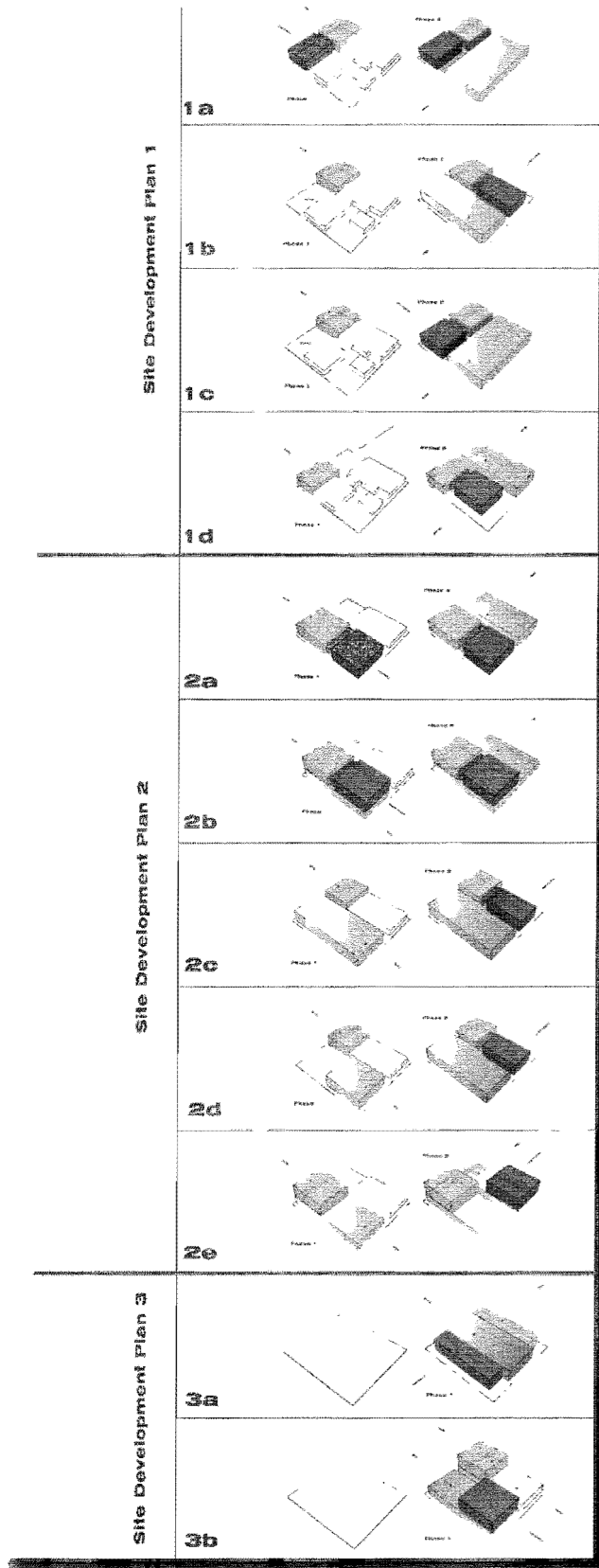
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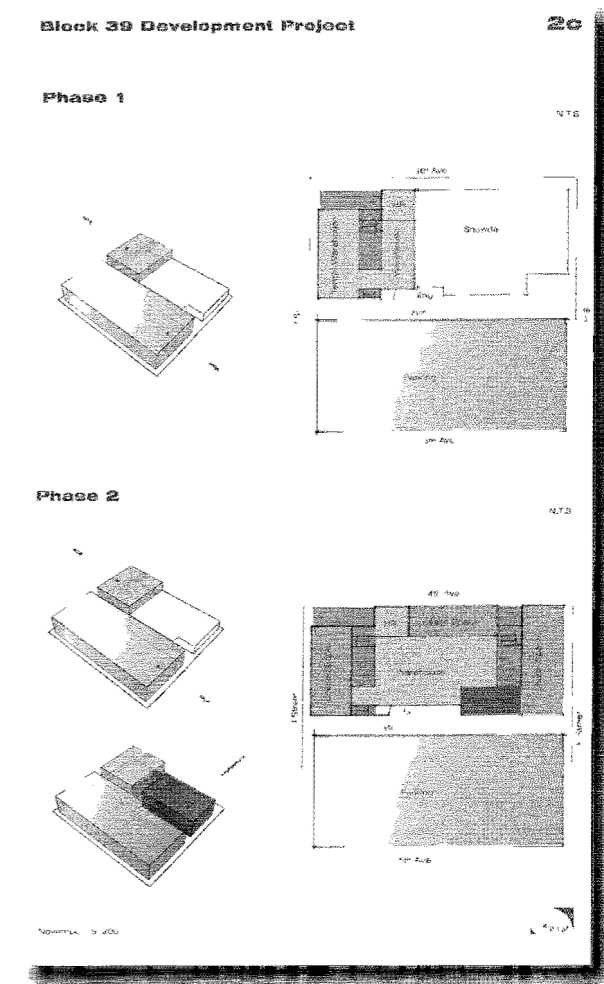
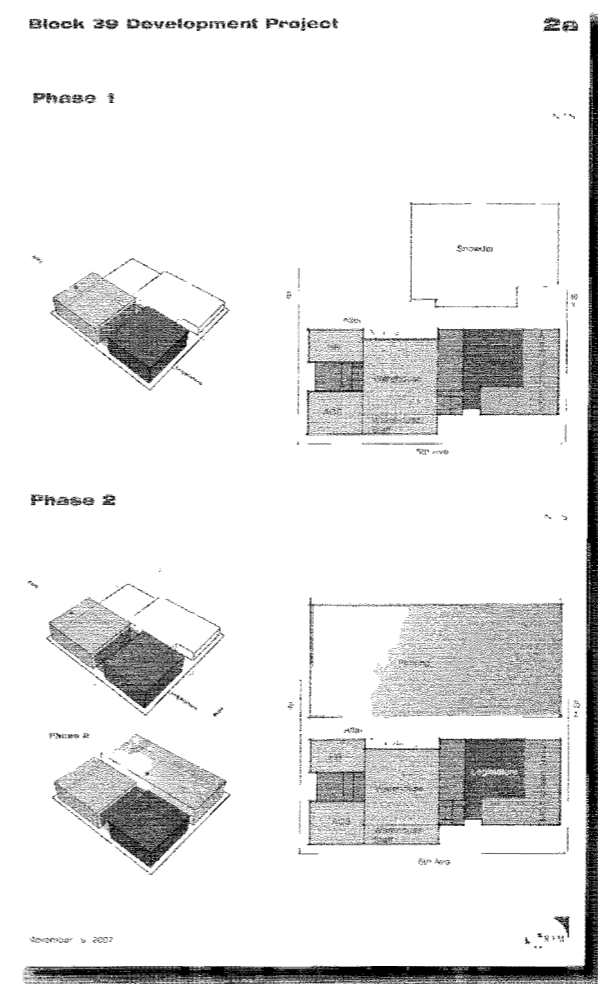
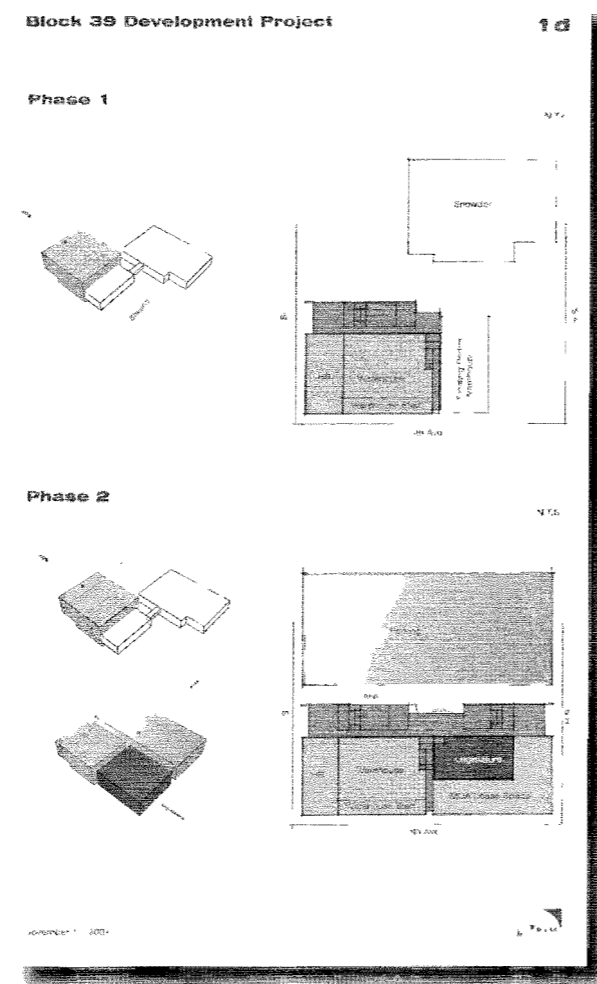
* Building Design to proceed as Core & Shell, for 35,000 NSF ASL, 65,000 NSF ACS, & 33,000 NSF Retail

- Interim Design Team meetings will be scheduled

Selected Schemes 1d, 2a and 2c from interim meeting following Design Workshop #1



Design Workshop #1: Eleven site development alternatives



- A municipal public process is typically required for construction of the project.
- To keep the concept design process on schedule, the stakeholders must agree on a single project design approach at the next workshop.

Subsequently, it became clear that it would be extremely challenging for the stakeholders to evaluate all the potential design alternatives for this project site and select the best solution in a single meeting. There are multiple potential design solutions for this project, and to competently select the best approach required the stakeholders to evaluate a wide range of criteria for all potential solutions.

Stakeholders decided that the best way to proceed would be for the design team and the stakeholders to have an interim design review meeting before the next scheduled Design Workshop to "shortlist" the best potential schemes.

At the interim meeting, the design team presented 11 multi-phased and single phased site development alternatives for Block 39 to narrow the potential options. The selection process was facilitated by the use of a "project criteria matrix" to apply key design criteria and allow the stakeholders to compare and grade the 11 alternatives. This process resulted in the stakeholders selecting the three best site development alternatives for further design development and final selection at Design Workshop #2.

Highlights of the short-listing process include:

- Any scheme which requires abandoning the alley and relocating the utilities was not selected due to the potential high cost and major disruption, and the negative affect of loss of the alley as an important urban amenity and "back of house" service area for the project.

- Any scheme which had the parking garage located under office space was not selected due to potential vehicle terrorism risk as well as the added higher cost of this building type.
- The project can potentially be constructed as a single phase or multiple phases. Single phase is most expeditious but disruptive due to the requirement to temporarily relocate existing Snowden office space and ACS computer systems located on the north half of the block during construction.
- After applying all project criteria, three phased scheme alternatives [schemes 1D, 2A and 2C] were selected for additional design analysis for Design Workshop #2. It was also agreed that each scheme will be analyzed as both a multi-phase and single phase project.

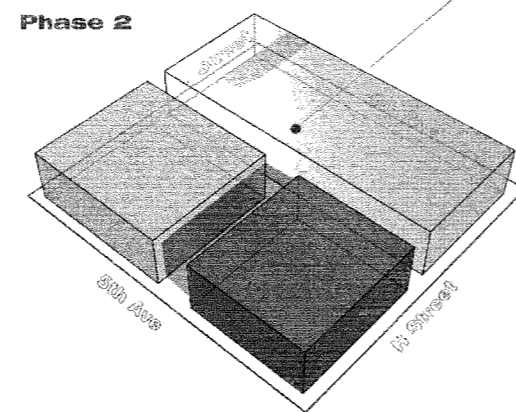
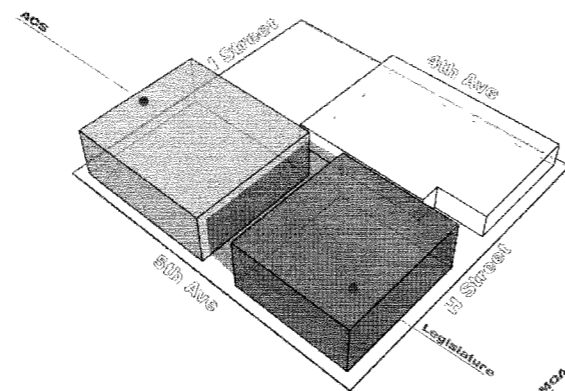
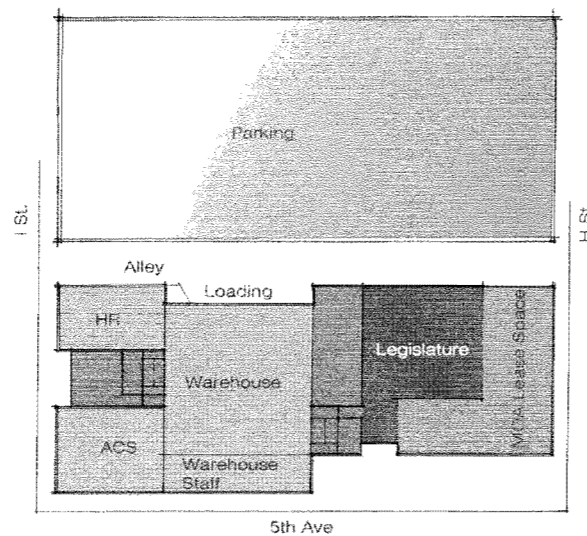
Design Workshop # 2

At Design Workshop #2, the design team presented each of the three alternatives and discussed preliminary building and urban design features, interim disruption to existing facilities and the surrounding neighborhood, cost ranges, construction schedules and constructability factors. After considering applicable criteria, the stakeholders selected scheme 2A as the preferred project solution.

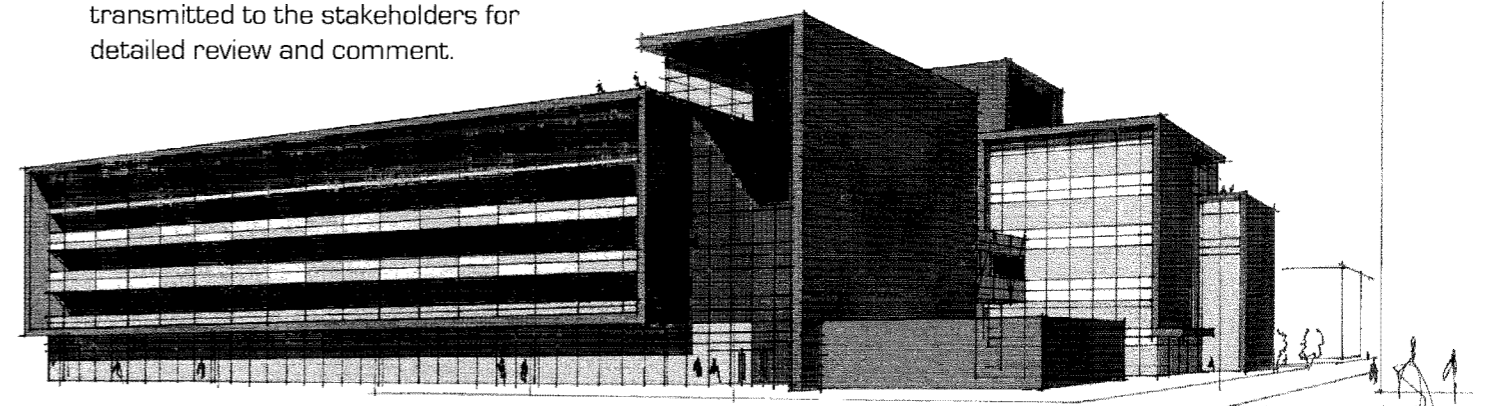
Highlights of Design Workshop #2 include:

- The selected scheme 2A is implemented by constructing the project in phases (office building on the south half of the block as an initial phase, parking garage and pedestrian sky bridges spanning the alley on the north half of the block as a second phase). It proved to be a balance considering schedule, construction feasibility, and interim disruption. It was also the most cost effective multi-phased scheme.
- Construction of the project as a single phase was found to be infeasible due to cost and disruption associated with disruption and interim relocation of Snowden Office Building occupants and computer systems. Construction of the office building on the south half of the site allows all ACS and ASL administrative systems to be relocated into the new building and be fully operational before the ACS Snowden building is demolished for the second phase parking garage. Interim parking solutions are available in downtown Anchorage.

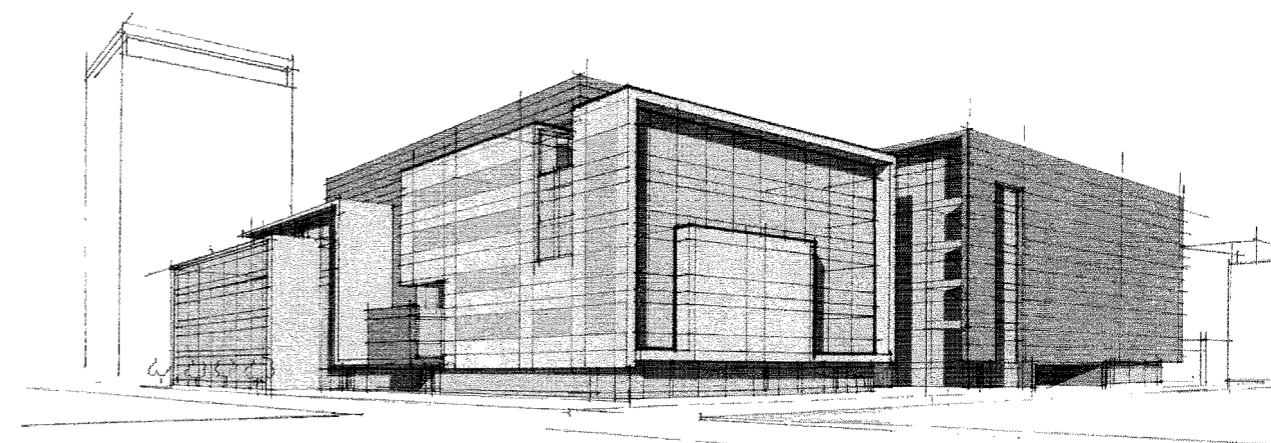
Design Workshop #2 Scheme 2A



- Due to first floor programming priorities and site constraints, it was necessary to locate a majority of the ACS warehouse and associated departments program in a basement level, with vertical circulation to the on-grade shipping and receiving area.
- It was agreed that physically separated and secured parking for the Legislature will occur at the upper level of the garage [approximately 80 spaces] with the addition of a roof structure to create covered parking.
- The stakeholders confirmed the necessity of having individual identities [separate entrances] and interior office environments for each stakeholder while providing a single set of building utilities to maintain cost effectiveness.
- Updated building interior layout programming and departmental stacking information was transmitted to the stakeholders for detailed review and comment.
- The existing property located at the northwest corner of the block, occupied by the Teriyaki Box Restaurant, will not be included in the overall site development. However, the parking garage concept will be expanded if the property can be acquired by the State prior to final design and construction.
- It was noted that on-grade lease space in both the parking garage and the office building would be considered a desirable urban amenity. In fact, a use that creates an appropriate amount of transparent storefront on-grade in the garage will be required by municipal ordinance.
- Although it was agreed that a third party will operate the parking garage, terms and conditions regarding the management of any available on-grade space in both the office building and garage remains to be finalized.



Project from intersection of 4th Avenue and I Street



Design Workshop #3 process sketch

Design Workshop #3

Design Workshop #3 was the presentation of a further developed concept design of the selected scheme, preliminary space plans and departmental layouts of all floors of the building, and the preliminary cost estimate and project schedule for stakeholder review and approval.

Highlights of Design Workshop #3 include:

- After reviewing a presentation of architectural drawings, interior space layout plans, and architectural models, the stakeholders agreed that the project design direction was generally acceptable and meets the goals and objectives for the project. The ACS program requirement for large assembly space on the 5th floor of the building triggers a code upgrade to the building construction type and a cost increase.
- The agreement between the stakeholders regarding management of on-grade available space

in both the office building and the parking garage remains unresolved and cannot be fully finalized until the full cost of the space is identified. It was clarified that individual "tenant improvement" costs for this type of space will not be included in this project's budget.

- Updated project cost estimate was presented. It was agreed that a follow on meeting with stakeholders and AHFC was appropriate to discuss strategies to reduce project costs without compromising functionality.

A post Design Workshop #3 project cost reduction meeting was held with representative stakeholders and AHFC to discuss potential strategies.

Cost reduction strategies discussed include:

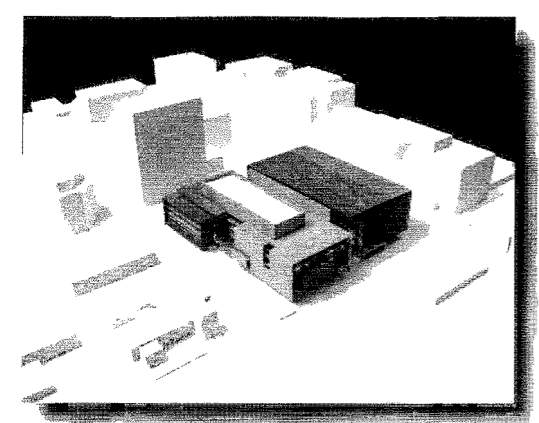
- Delete the roof from the upper level of the parking structure, thus eliminating covered parking on

the upper parking deck. This change assumes that legislative parking previously planned to be secured on the upper deck of the building, would be shifted to a lower level of the building, and would be reserved but not physically secured.

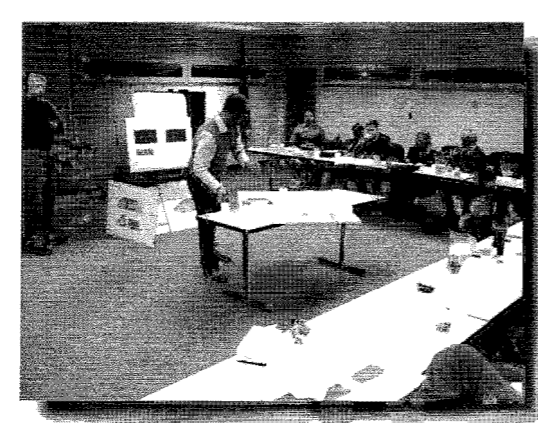
- Delete the basement level of the office building, and move the ACS warehouse and associated program space from the basement to on-grade space under the parking garage, and on-grade space within the first floor of the office building. This change would replace on-grade lease space in the garage with ACS office and storage space.
- Move large conference rooms from the 5th floor of the building to a lower floor. This change would allow a reduction in the building construction cost.
- Replace on-grade available space in the ASL portion of the office space with ASL departments from upper levels of the building.

- Consolidate/rearrange building program space without compromising functionality to allow overall office building area reductions.
- Use cost effective building finish materials that provide good life-cycle performance for a good quality utilitarian downtown office building.
- In addition to design changes to reduce cost, ACS will work with AHFC to evaluate alternative strategies for project financing.

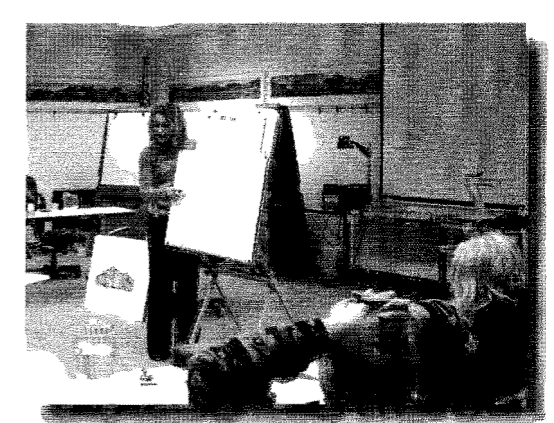
The cost reduction design changes were integrated into the project and presented to the stakeholders in a follow up session along with an updated cost estimate. Anticipated project cost had dropped from approximately \$108 Million to approximately \$86.5 Million. Although some additional evaluation of interior space planning was still required, it was generally agreed that the project design and reduced budget was feasible. Approval was given to the design team to finalize their concept design work product.



Process Model #3



Design Workshops in progress



Public Process

There are several aspects to this development which will require the project to go through public review processes. These include:

Platting

The five existing lots within the block will need to be re-platted into two lots. The local utilities and governmental agencies will review the proposed new property and easement layout.

Public Facility Site Plan Review, & Public Facility Project Landscaping Review

Since the proposed building will house State of Alaska agencies, the project will go through the MOA "Public Facility" reviews.

Conditional Use Permit

Since the parking garage will contain more than 50 spaces, its use is considered a 'conditional use' in the B-2B Zoning District. The project will be submitted to the MOA Planning Department, Planning & Zoning Commission [public hearing], and to the local community council for review.

Bulk Variance

Since the gross square footage of the garage will be greater than 26,000 square feet, a variance from the specific bulk dimensions will be required.

Bonus Points

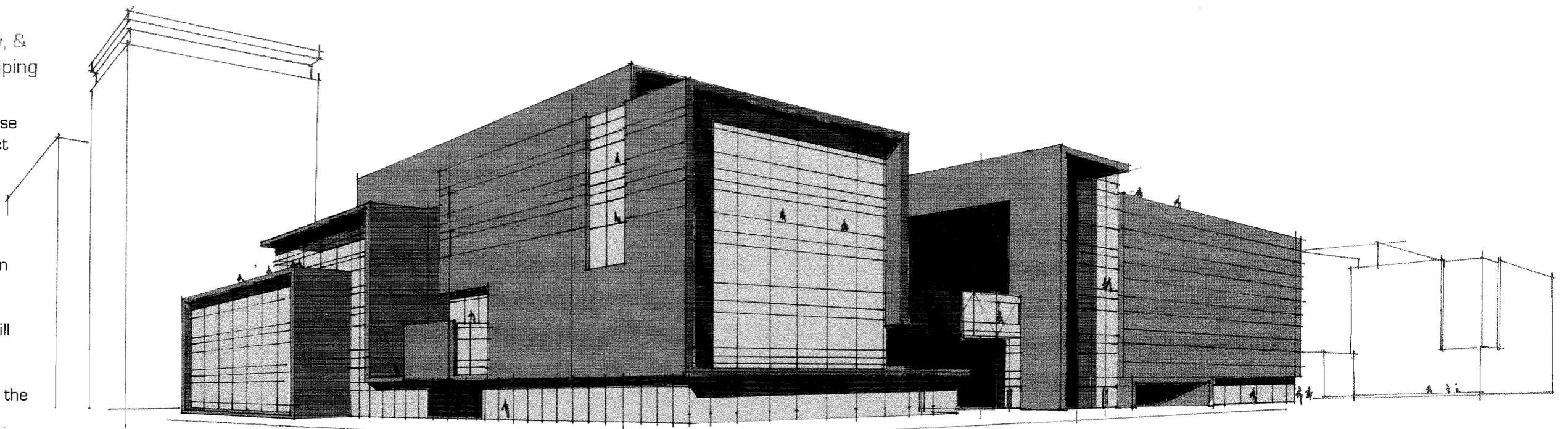
This project secures bonus points by incorporating heated sidewalks, textured sidewalks, benches, street trees, pedestrian-scale lighting, bike racks and sky bridges.

Urban Building Design

Due to the unique design requirements of the office building, the design will be submitted to the director of MOA Community Planning & Development for approval. Through this process [Alternative Structure Design Process], the project must demonstrate that at least 15 percent more access is provided either to scenic views or for solar access, than compared to a design meeting the zoning requirements.

Traffic Analysis

According to State DOT&PF Driveway Standards, a development that creates more than 100 new trips during the peak hour will trigger a Traffic Impact Analysis (TIA) of the site driveways and roadway system surrounding said development. The design team conducted an initial scoping meeting with MOA and DOT&PF; based on ACS, legislative staff or other public users. It was noted that an abbreviated traffic report will likely be necessary to analyze the shift in downtown traffic.



View from 5th Avenue and H Street

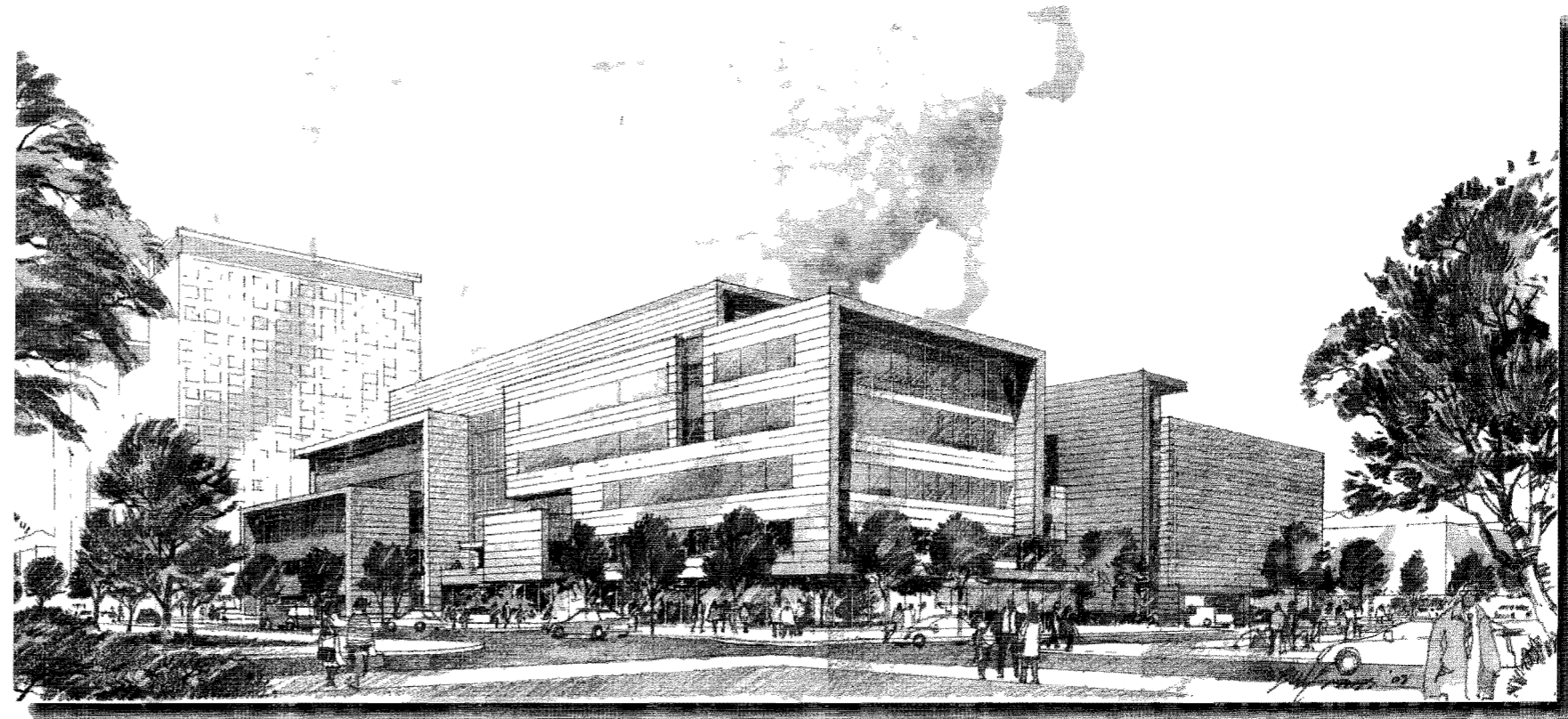
Concept Design

The Final Concept Design for this project consists of a five-story office building, located on the south half of Block 39, to house the ACS and ALO. On most of the north half of the block will be a seven-level parking garage for use by the court system and legislature staff, as well as jurors. The parking garage provides 491 parking spaces. The office buildings and parking garage are connected by sky bridges that span across the alley.

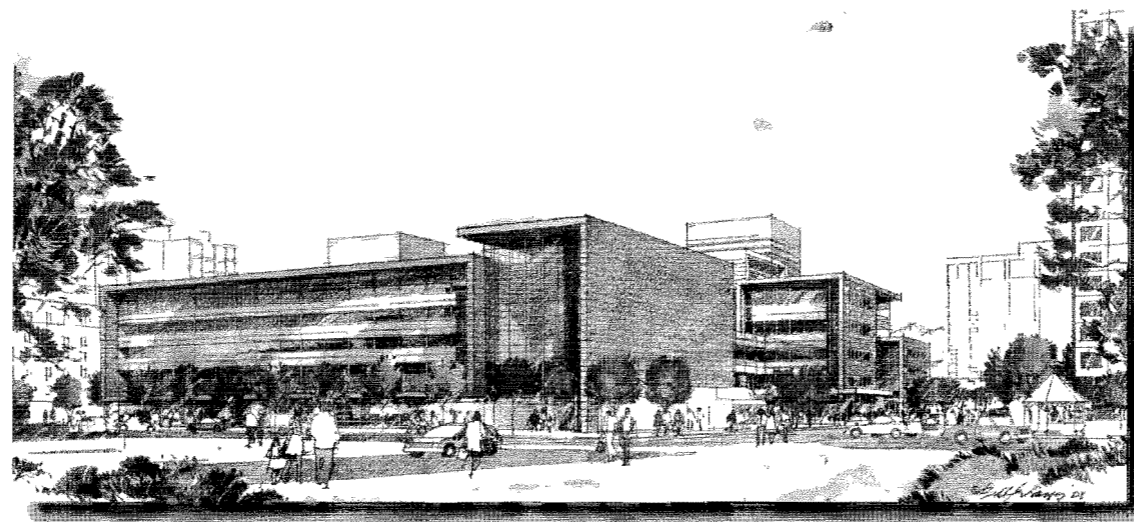
The ALO are located in the eastern portion of the building, with their entry on the south side. The court system offices will occupy the western portion of the building, with their own entry on the west side of the structure. These entries open to separate lobbies and elevators. The architecture of the building provides an individual expression for each stakeholder.

The parking garage is oriented with the vehicle entry/exit located on the east side, of H Street. The ground level of the garage will consist of court system office and storage space and building support areas. From the entry/exit point, a 'speed ramp' leads up to the second level where parking begins. The upper levels of parking continuously spiral up, with the ramped portions being located on the alley side. The parking decks will be level on the 4th Avenue and H Street sides. Vertical circulation elements are located on the northwest and southeast corners, and are architecturally expressed with glass curtainwall enclosure.

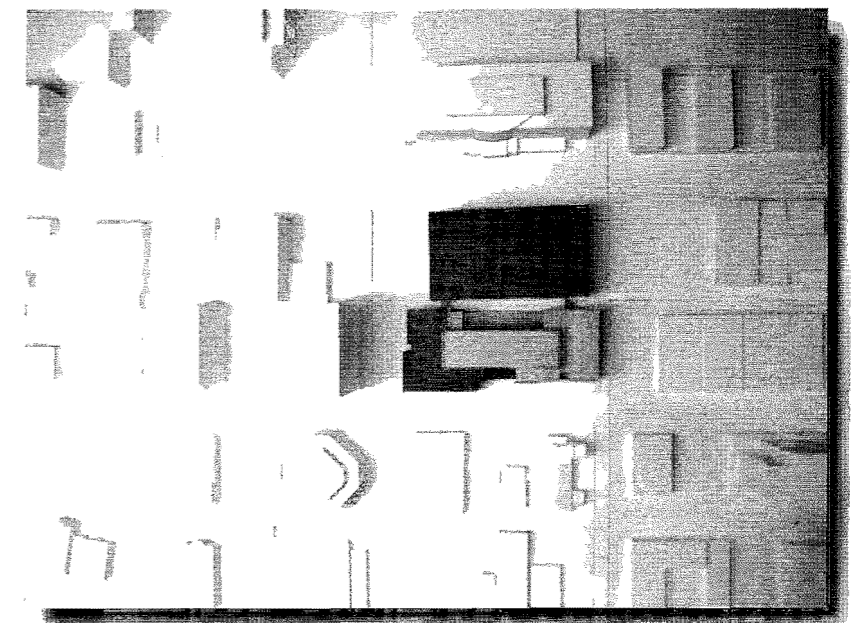
The following sheets provide more detailed information on the site plan, building floor plans, and building exterior elevations.



Project rendering view from 5th Avenue and H Street showing office space



Project rendering view from 4th Avenue and I Street showing parking garage



Site Study Model

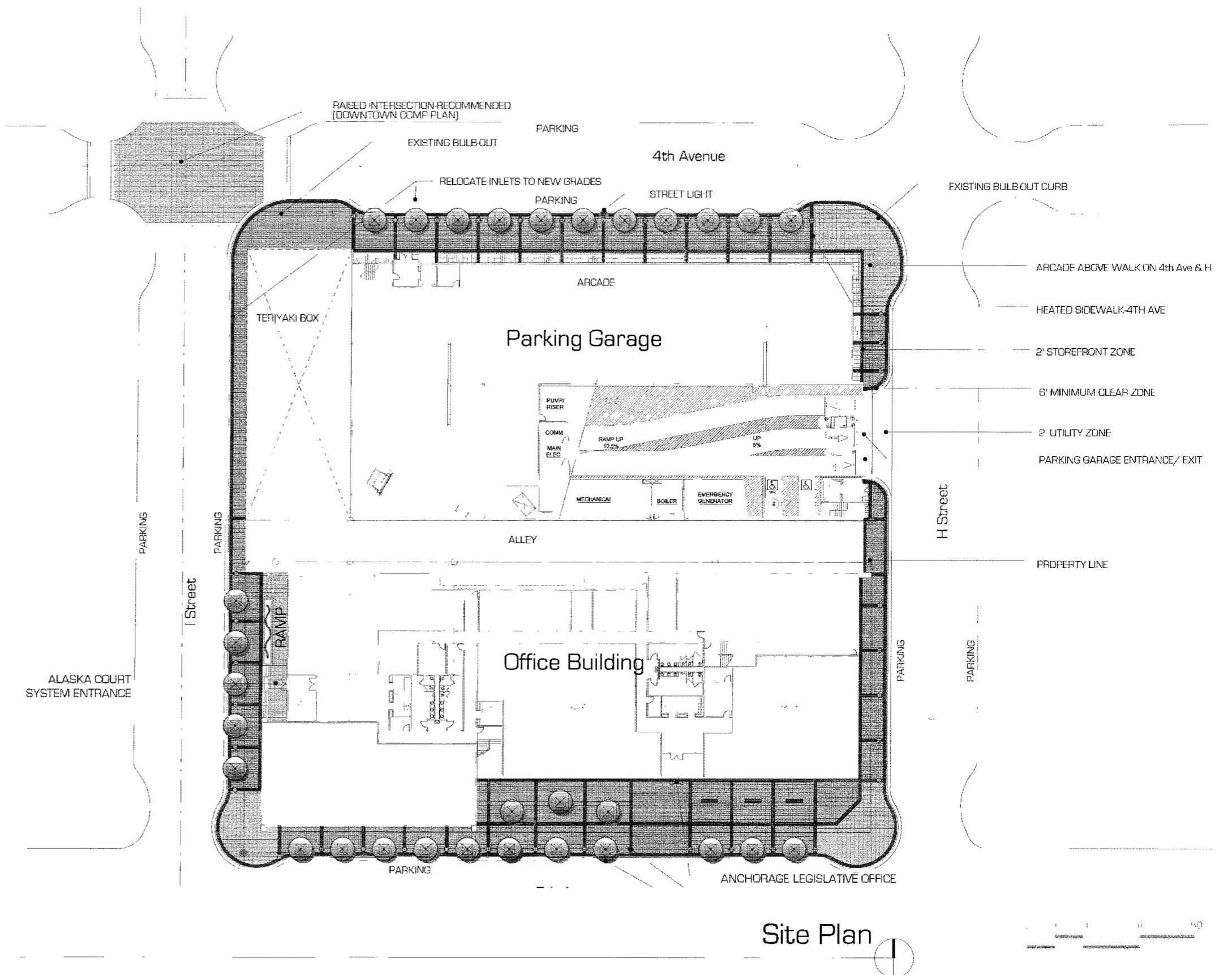
Site Plan

The site will be wrapped with new broader and pedestrian-friendly sidewalks, including new curb bulbs along 5th Avenue. Sidewalk surfaces will be finished using exposed aggregate in the large fields and a light broom finish on banding. Wider, plaza-like space is provided on the west and south sides, the ACS and AOL entries respectively. The sidewalks on 4th Avenue will be further enhanced by the lower level of the parking garage being set back from the levels above, creating a covered arcade against the building.

The building service side of both buildings will be located along the alley; including the loading berths, trash dumpsters, and utility meters. The alley will be repaved to improve the ability to remove snow and maintain the service area.

The established pedestrian-scale lighting along 4th and 5th Avenues will be retained along these frontages as a part of this project.

Street trees will be provided on the north, west and south sides of the block, where sidewalk widths are generous. Additional site amenities will include benches near the ALO entrance, bike racks at the garage, planters at the entrances and litter receptacles at the four corners of the block. Several of these amenities are provided to earn bonus points required of the development.

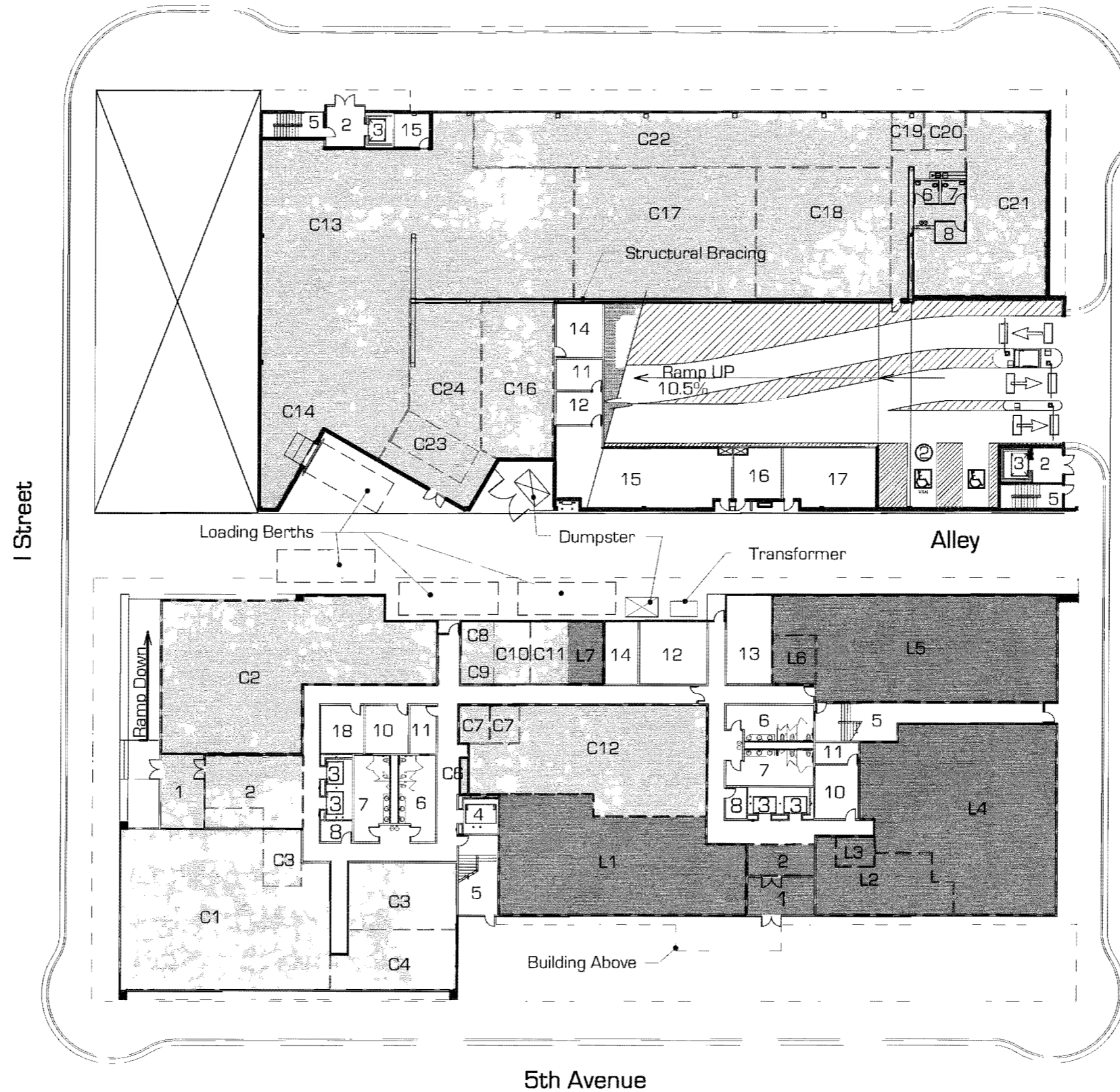


Block 39 Development	conceptual design
Office Building and Parking Garage	
Site Plan	1.11.08

4th Avenue

BUILDING LEGEND

1. Vestibule
2. Lobby
3. Elevator
4. Service Elevator
5. Stairs
6. Women Restroom
7. Men Restroom
8. Janitor
9. Mechanical Shaft
10. Electrical Room
11. Communication Room
12. Main Electrical Room
13. Main Communications Room
14. Water Riser/Sprinkler Riser Room
15. Mechanical Room
16. Boiler Room
17. Emergency Generator Room
18. Recycling



ACS

- C1. Human Resources
- C2. Records Management
- C3. Information Services Storage Room
- C4. Training Center Conference
- C5. Not Used
- C6. Coffee Bar
- C7. Shower
- C8. Mail Sorting
- C9. Bike Storage
- C10. Maintenance
- C11. Staging
- C12. Archives
- C13. Main Warehouse
- C14. Staging/Loading Dock Area
- C15. Warehouse Office
- C16. Records Management
- C17. Small Supply Warehouse
- C18. Forms Area
- C19. Forms Office
- C20. Printshop/Forms Counter
- C21. Printshop
- C22. Youth Court Offices
- C23. Mailroom
- C24. Available Space

ALO

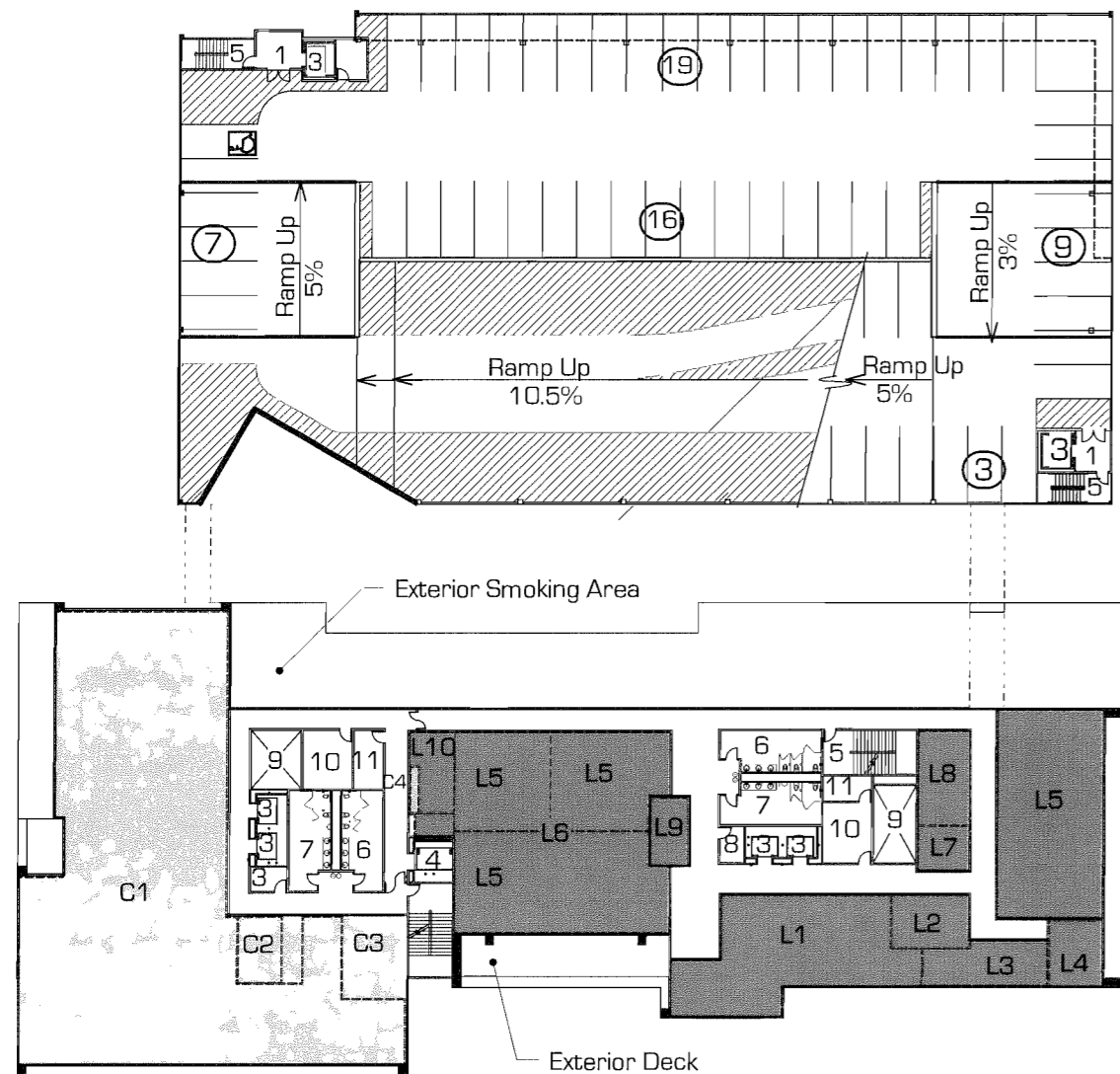
- L1. Ombudsman
- L2. Legislative Ethics
- L3. Legislative Security Office
- L4. Legislative Audit
- L5. Victims Rights
- L6. Legislative Building Maintenance
- L7. Staging

Ground Level Plan

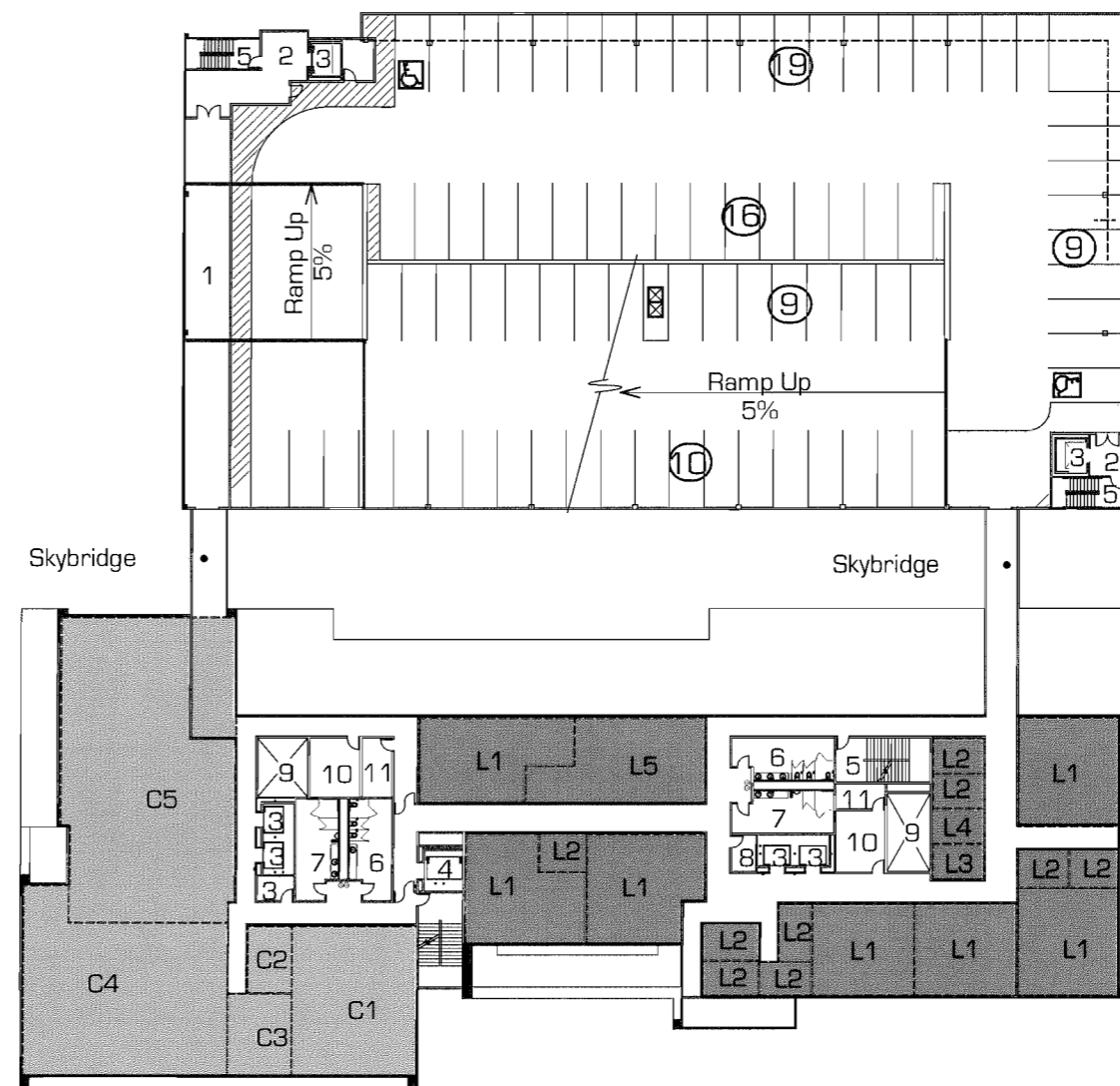


Block 39 Development
Office Building and Parking Garage | conceptual design

Floor Plan 1 | 1.11.08



2nd Level Plan



3rd Level Plan

BUILDING LEGEND

- 1. Garage Passageway
- 2. Lobby
- 3. Elevator
- 4. Service Elevator
- 5. Stairs
- 6. Women Restroom
- 7. Men Restroom
- 8. Janitor
- 9. Mechanical Shaft
- 10. Electrical Room
- 11. Communication room

2nd Level ACS

- C1. Information Services
- C2. Open Stair
- C3. IS Shared Conference Room
- C4. Coffee Bar

2nd Level ALO

- L1. LIO Offices
- L2. Copy/Mail
- L3. Library/Conference
- L4. Conference
- L5. Medium Conference
- L6. Large Conference [Fully open]
- L7. Kitchen
- L8. LIO Storage
- L9. Teleconference Moderator Room
- L10. LIO/Conference Storage

3rd Level ACS

- C1. Information Services Department, CI
- C2. Open Stair
- C3. Facilities Shared Conference Room
- C4. Facilities Department
- C5. Fiscal Department
- C6. Coffee Bar

3rd Level ALO

- L1. Typical Rep Suite
- L2. Office
- L3. Copy/Work Area
- L4. Kitchen
- L5. Data Processing

The building steps back beginning at the second level on the north side of the building and on part of the south. There will be a heated exterior deck on the south side where this step occurs which will be accessible from the ALO's conferencing center. The step on the north will provide greater relief from the alley and parking garage to the north, allowing for more potential natural light to the offices on the north side of the building.

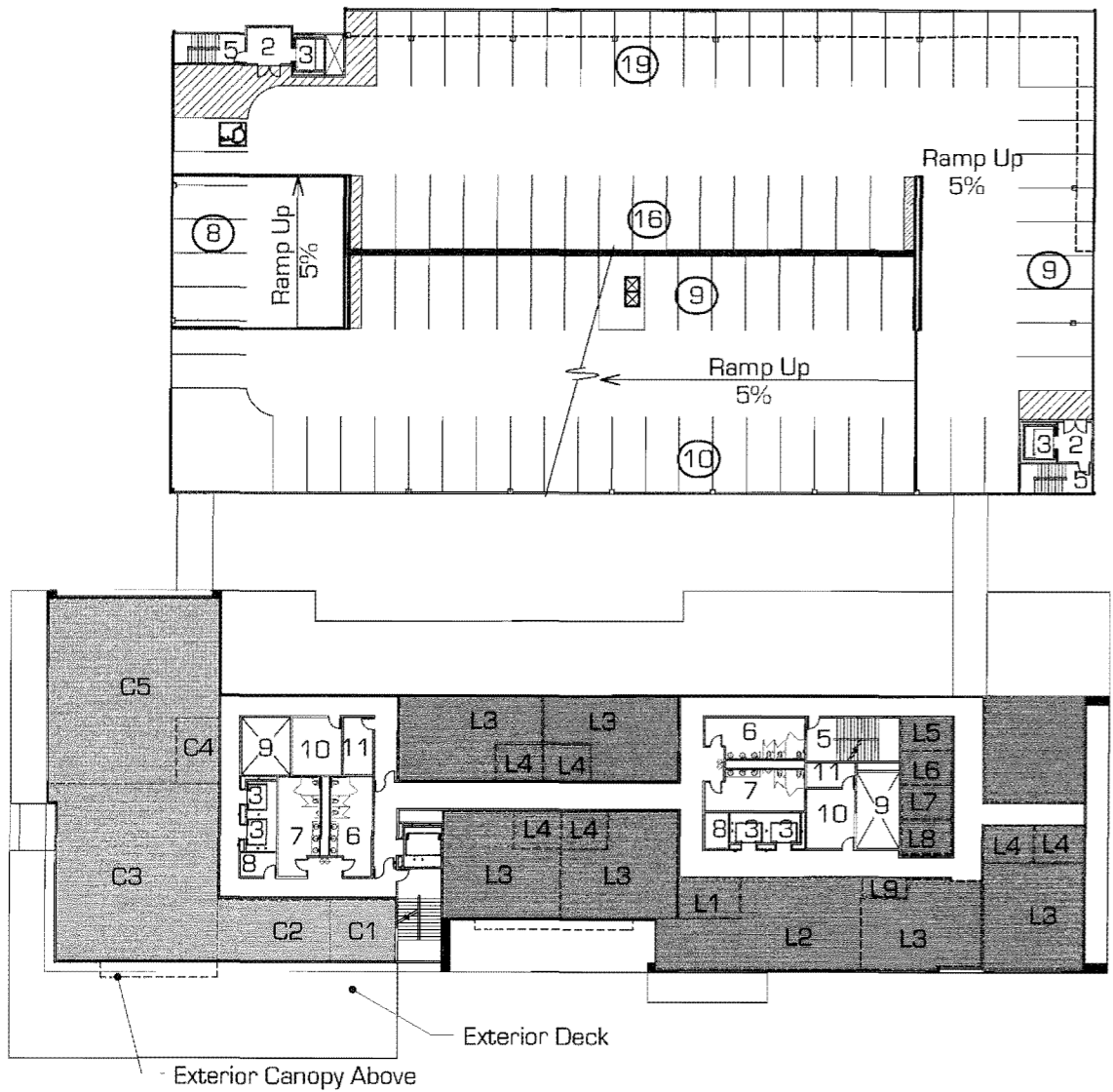
The office building is connected to the parking garage at the third level of the office and garage buildings via sky bridges.

The upper levels of the garage are organized with two vertical pedestrian circulation cores at the northwest and southeast corners. This parking garage configuration maximizes efficiency for vehicle ingress and egress. It recognizes that the destinations for the court system employees and visitors are at the west side of the block, and the destination for employees and visitors to the

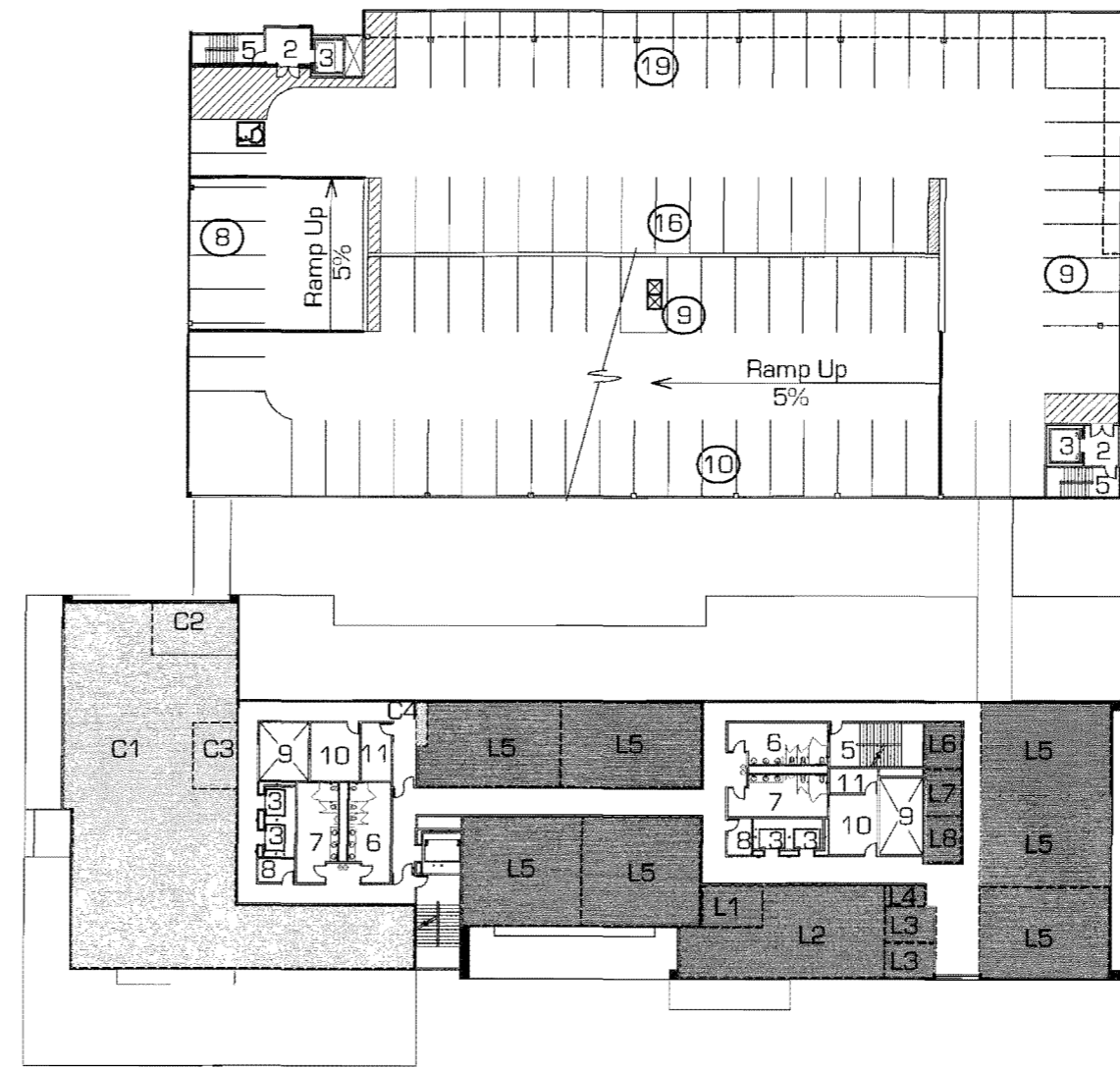
legislative office are the southeast corner of the block. Accessible parking spaces are distributed at the two pedestrian cores.

There are approximately 89 parking spaces on a typical upper level of the garage. The total 491 spaces in the garage are divided into 120 spaces for the ACS, 100 spaces for the ALO, and the balance for juror parking.





4th Level Plan



5th Level Plan

BUILDING LEGEND

- 1. Vestibule
- 2. Lobby
- 3. Elevator
- 4. Service Elevator
- 5. Stairs
- 6. Women Restroom
- 7. Men Restroom
- 8. Janitor
- 9. Mechanical Shaft
- 10. Electrical Room
- 11. Communication room

4th Level ACS

- C1. Small Conference Room
- C2. Training Center Support
- C3. Training Center
- C4. Open Stair
- C5. Deputy Director's Department

4th Level ALO

- L1. House Majority Press Secretary
- L2. House Speaker Suite
- L3. Typical Representative Suite
- L4. Office
- L5. House Minority Press Secretary
- L6. Webmaster
- L7. Kitchen
- L8. Copy/Work Area
- L9. Media Room

5th Level ACS

- C1. Deputy Director's Department, Legal
- C2. Legal Shared Conference Room
- C3. Open Stair
- C4. Coffee Bar

5th Level ALO

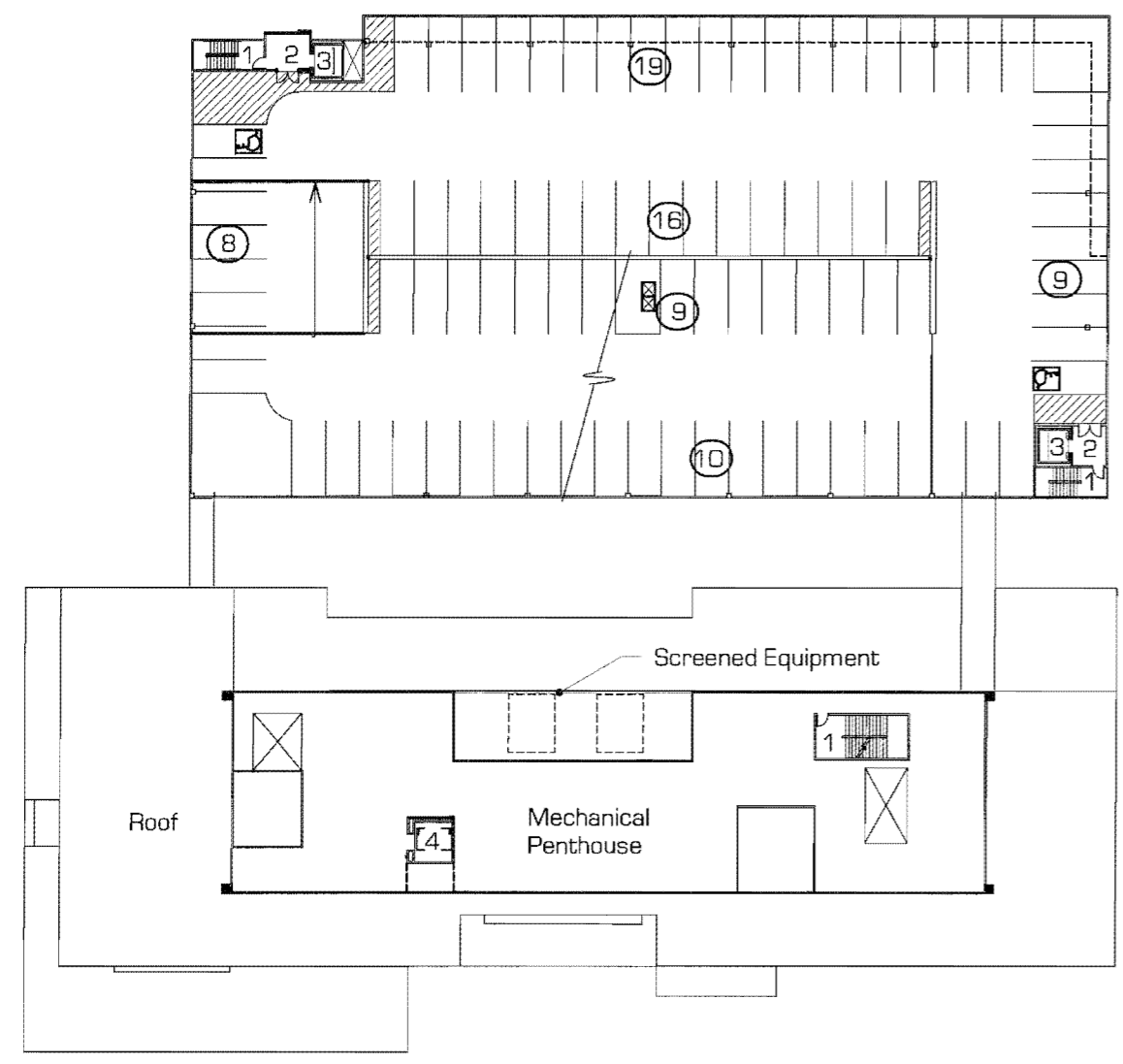
- L1. Senate Majority Press Sec.
- L2. Senate President Suite
- L3. Office
- L4. Media Room
- L5. Typical Senator Suite
- L6. Senate Minority Press Secretary
- L7. Kitchen
- L8. Copy/Work Area



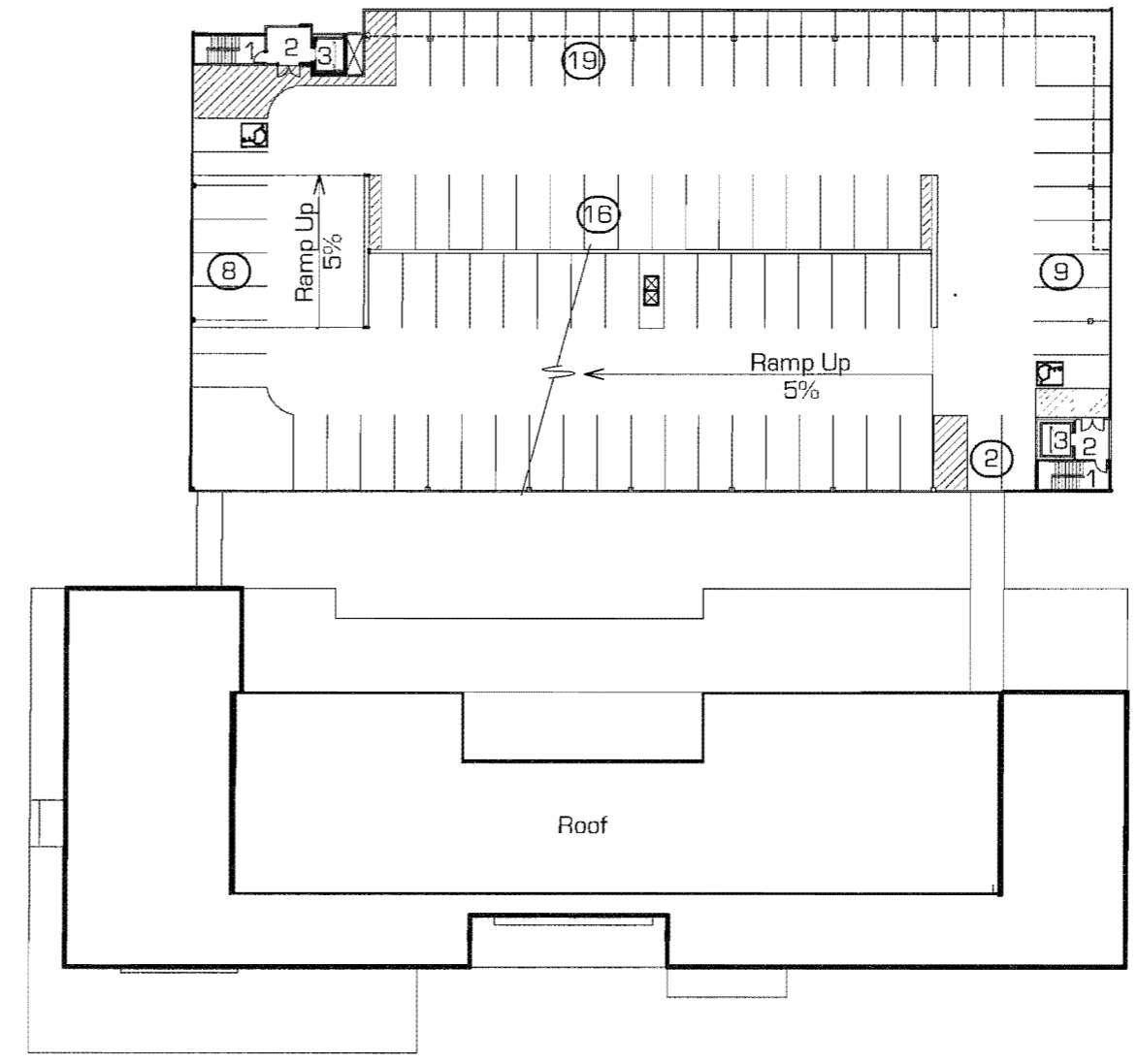
The building steps back on the south and west face of the fourth floor to provide a heated exterior deck which will be accessible from ACS conferencing center.

BUILDING LEGEND

- 1. Stairs
- 2. Lobby
- 3. Elevator
- 4. Service Elevator



6th Level Plan



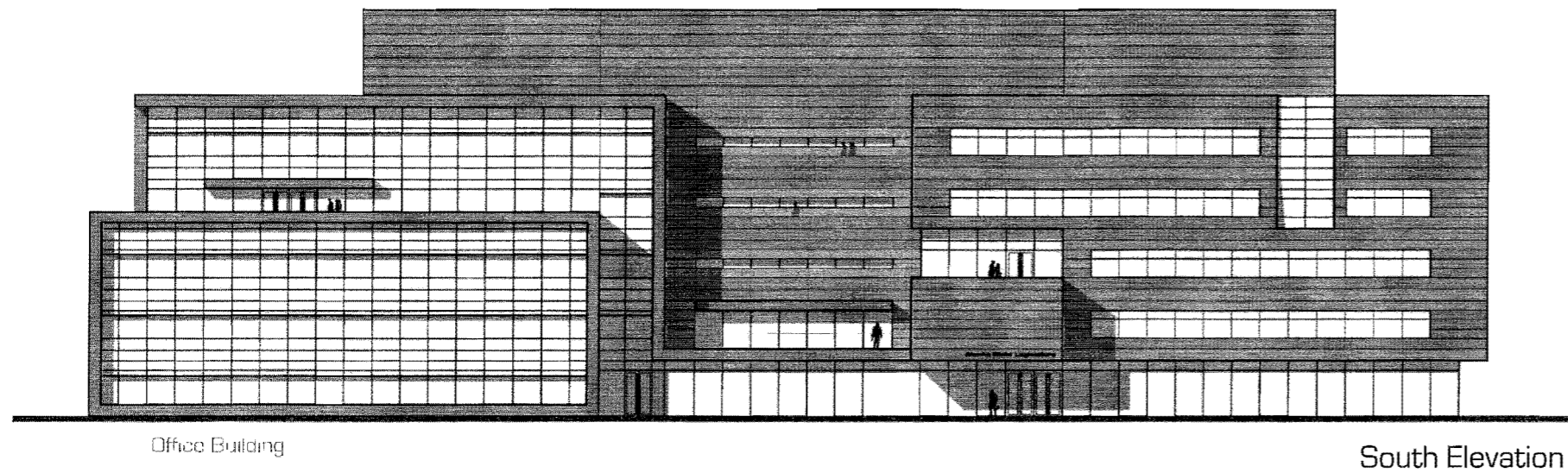
7th Level Plan



The mechanical and electrical systems for the office building will be split and serve the court system and legislative office separately. The air-handling equipment and boilers will be located in the penthouse. Air conditioning chillers will be located on the roof, behind a screen wall. The service elevator will extend up and stop on this level, to allow repair/replacement of large pieces of equipment.

Block 39 Development
Office Building and Parking Garage | conceptual design

Floor Plan 6 & 7 | 1.11.08



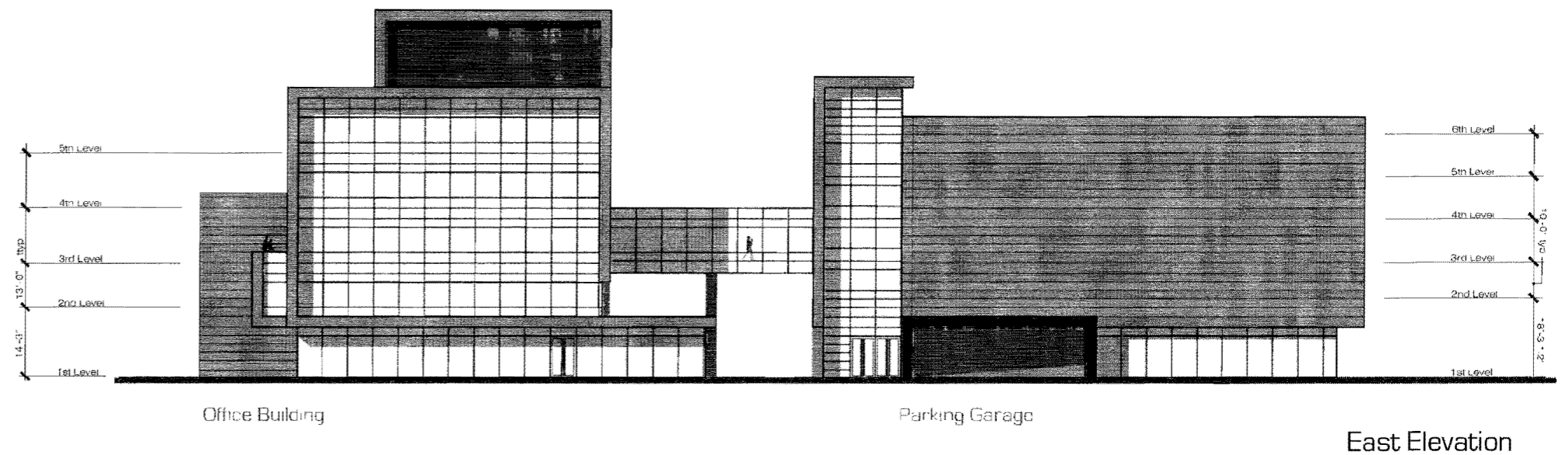
Exterior Elevations

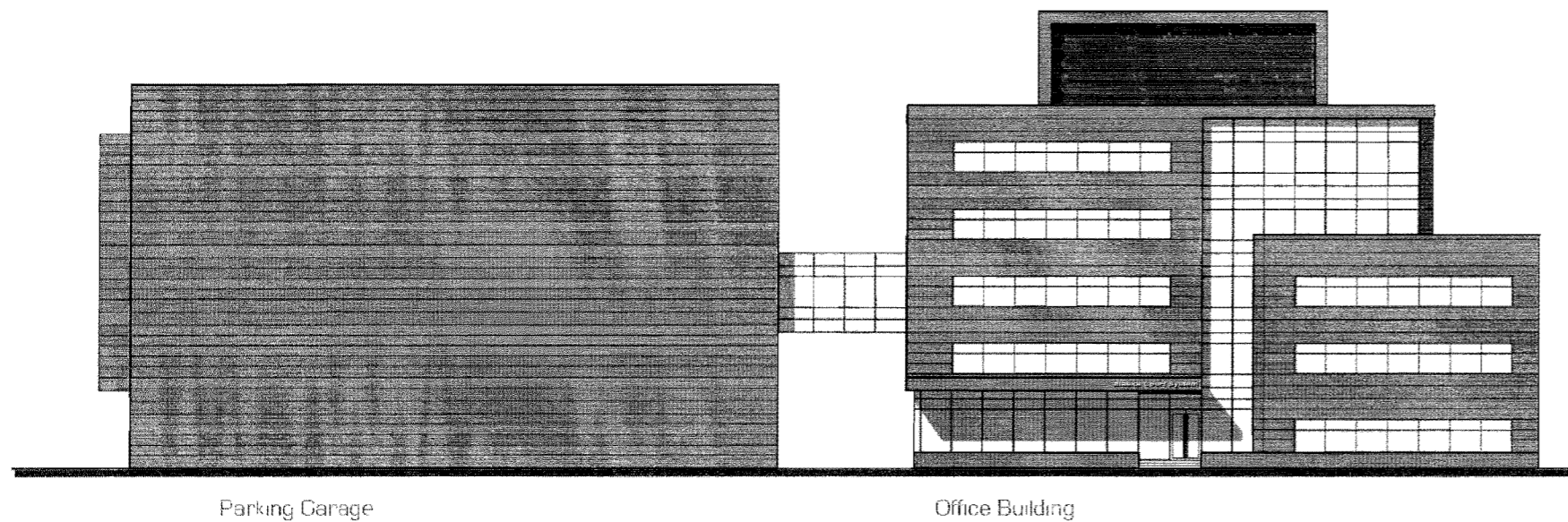
One of the objectives was to create a single office building with two different identities, one for each stakeholder.

By using a limited palette of materials, and having a single strategy for massing these materials, we were able to achieve this objective, as well as significantly reduce the overall bulk of this building. The parking garage on the north will include both the material use and composition to further reinforce the entire block as a single civic project.

Alaska Court System: Located on the south-west corner of the site, the north and south faces will be mostly curtain wall, the west elevation being primarily stone or metal panel with punched windows mixed with some curtain wall elements. The entry will be located on the west side of the building with direct visual connection to the courthouses.

Anchorage Legislative Offices: Located on the south-east corner of the site, the north and south faces will be primarily stone or metal panel with individual windows, with the east elevation being curtain wall. The entry will occur on the south side of the building.

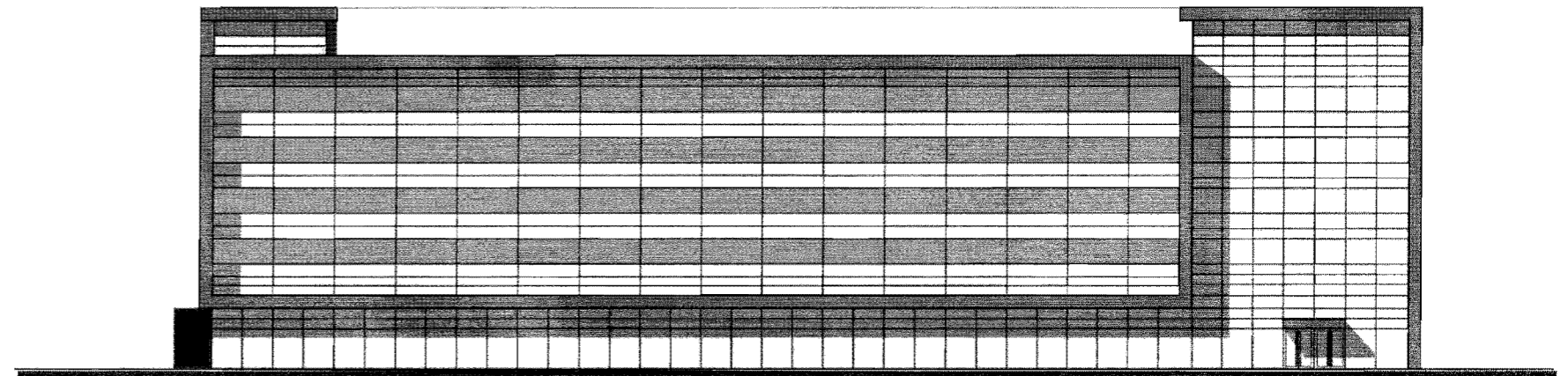




Parking Garage

Office Building

West Elevation



Parking Garage

North Elevation

Parking Garage: Located on the north half of the block, the parking garage facade will be an "urban friendly" combination of metal panel and curtain wall. The north elevation will have curtain wall at the ground level which will contain office space. The floors above will be framed by metal panels which will cantilever out over the sidewalk providing a pedestrian friendly covered walkway. The exterior material on the parking levels above will be a curtain wall system with spandrel panels and openings for ventilation. On the west end of the south elevation will be a vertical circulation tower that will be glass curtain wall. The

east elevation will be glass at the ground level with metal panel at the parking levels. On the south end of the east elevation will be another vertical circulation tower that will be glass curtain wall. The south elevation of the garage will be primarily metal or concrete panels with openings for ventilation and loading/back of house access at ground level. The west elevation, a fire wall, will be clad with metal panels. Sky bridges connecting the garage to the office buildings will be glass.

Building Structure. The foundation of the office building will consist of cast-in-place concrete on compacted soils. The ground floor will be concrete slab-on-grade. The superstructure of the building will be steel columns and beams and the upper floors will be concrete filled metal deck. Seismic bracing will be located at the interior cores of the building.

The parking garage will be of similar construction; concrete foundation, steel columns and beams, and decks of concrete filled metal decking.

Block 39 Development
Office Building and Parking Garage | conceptual design

Building Elevation | 1.11.08

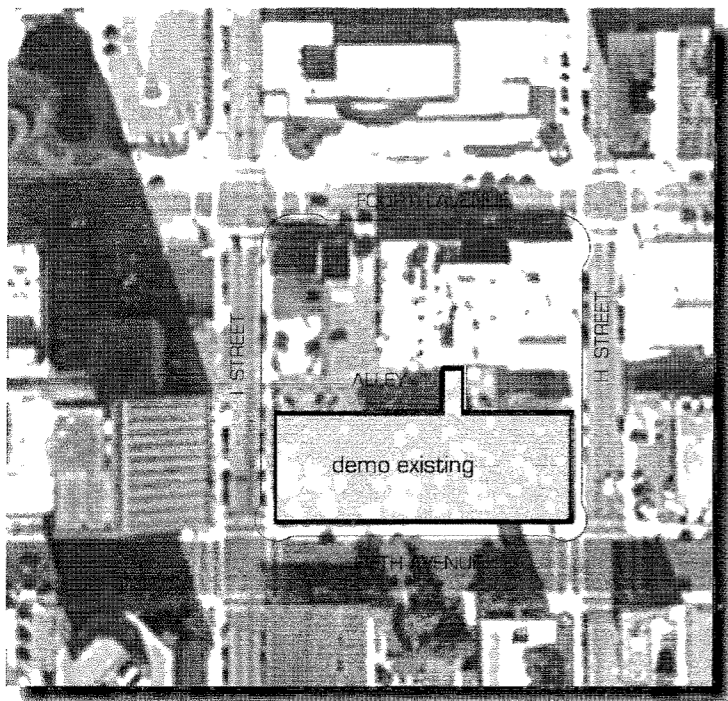
Phasing Sequences

The Block 39 Development Project has been planned to proceed with a construction sequence that would allow several efficiencies to be realized. Construction season timing for a project of this magnitude has a significant effect on both the design and construction schedules.

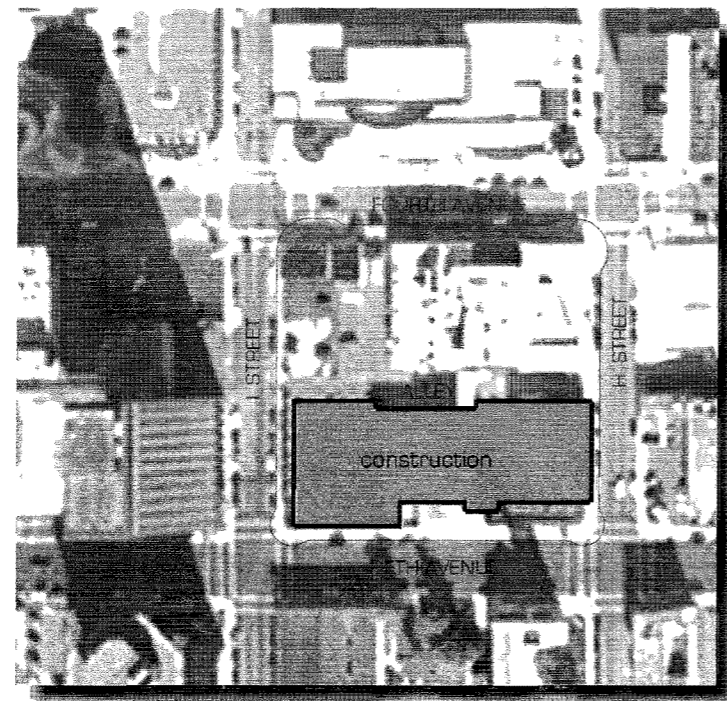
The procurement is scheduled to occur by late spring of 2008, to allow the design of the office building and parking garage to be completed just after the end of 2008. The design schedule includes interim deliverables to allow the public review process to be completed by end of the year, and for the early procurement of long-lead-time construction components.

The first phase of construction will be the office building on the south half of the block. Demolition of the existing buildings and construction of the new office building will begin in January 2009. This allows the office building to be occupied in early April 2010, which is also the critical start time to commence construction of the parking garage structure – the second phase of the project.

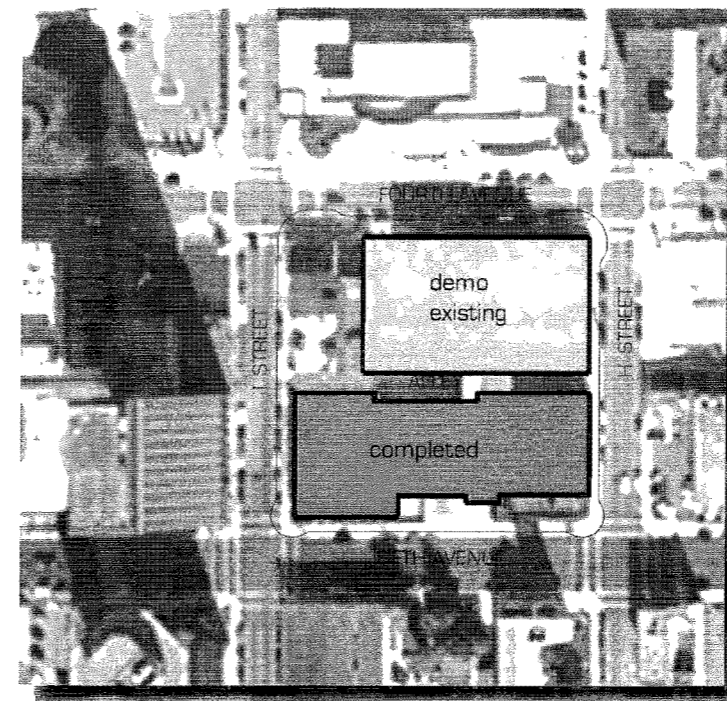
Stakeholders will relocate to the new office building, and the existing buildings located in the north half of the block will be vacated and demolished. This schedule accommodates construction of the parking garage in the most efficient season, April through October. By starting the garage in spring of 2010, this allows overall project completion in summer of 2011.



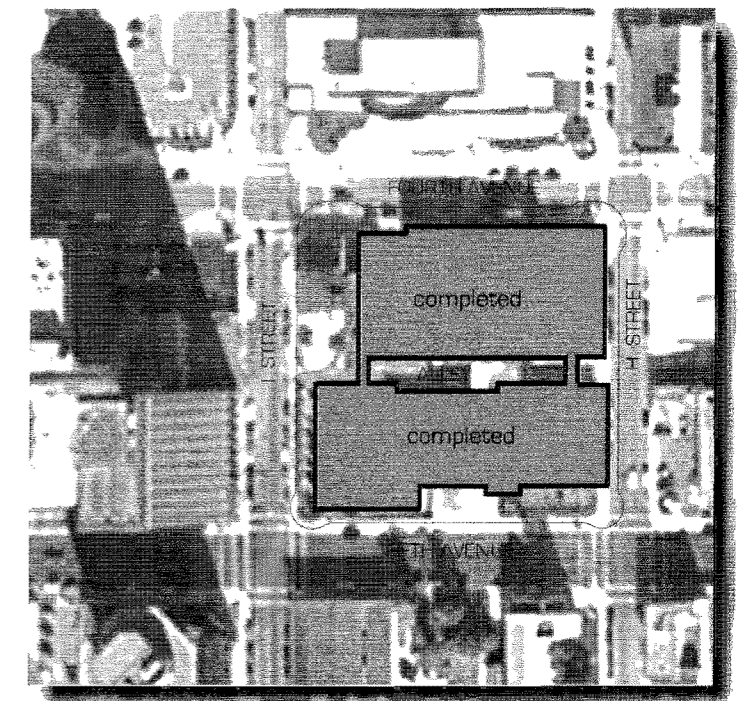
Phase One Demolition



Phase One Construction



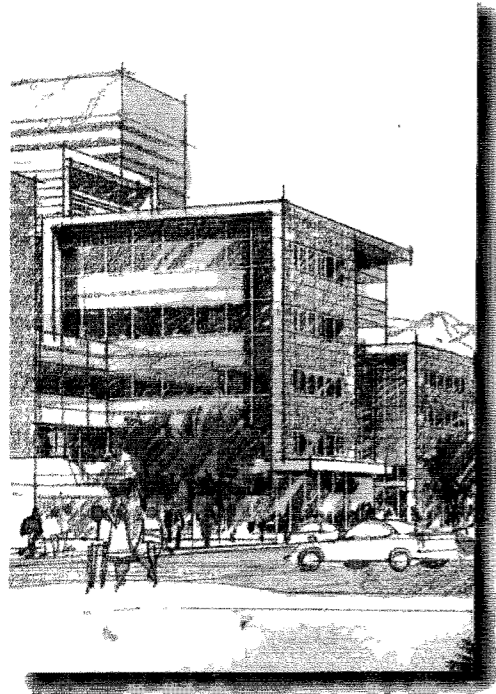
Phase Two Demolition



Phase Two Construction

Total Project Cost

\$86,517,000



Financing

During the first half of the Twenty-Fifth Legislative Session, legislation was introduced to authorize funding for the project. The committee process will continue during the second half of the legislative session beginning January 2008.

AHFC provided information concerning the cost of financing the project, as follows:

Finance Alternative #1

Assumptions:

- \$86.5 Million
- 20 year debt
- Current rates + 1/2%
- Normal closing costs

Annual Debt Service: \$7,000,000

Finance Alternative #2

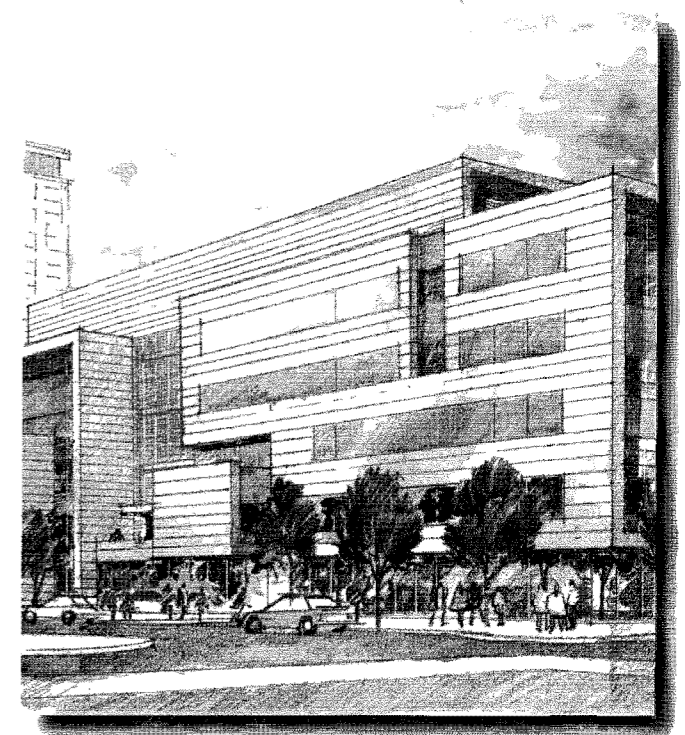
Assumptions:

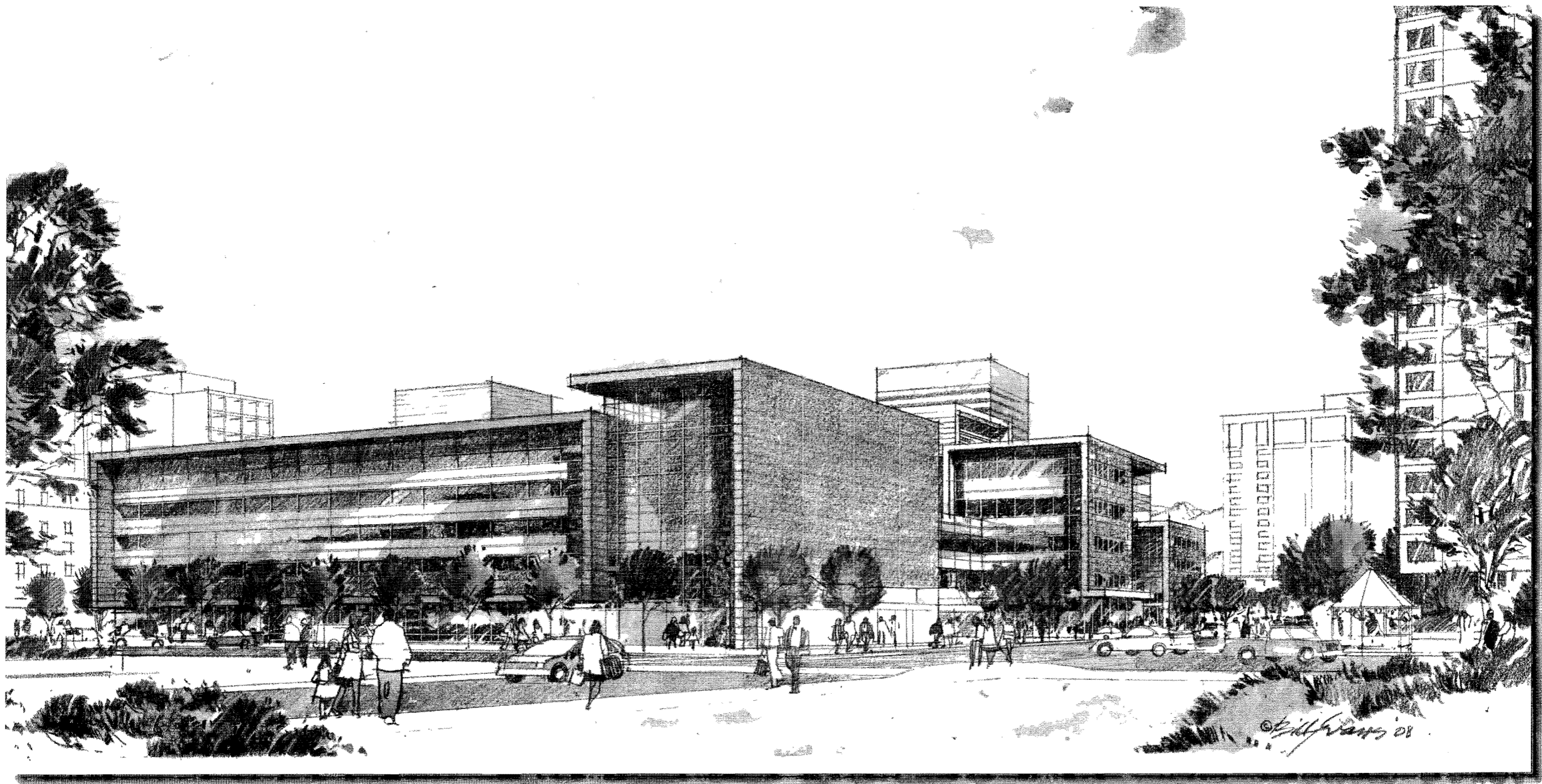
- \$86.5 Million
- 25 year debt
- Current rates
- Normal closing costs

Annual Debt Service: \$5,600,000

These numbers are both somewhat more conservative than represented because they assumed no reinvestment income from the proceeds of the bonds. There will, of course, be some earnings that will be contributed to the Debt Service Reserve Fund and offset either the early years' debt service or reduce the amount that needs to be borrowed.

The Legislature currently has operating funds for their space leases and the court system has operating funds to maintain the Snowden buildings. These funds can be applied toward the operation of the new facility once constructed.





Block 39 Development

